



## **Council Meeting**

Jubilee Centre Council Chamber  
9909 Franklin Avenue, Fort McMurray

Tuesday, August 26, 2014  
6:00 p.m.

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### **Agenda**

#### **Opening Prayer**

#### **National Anthem**

#### **Call To Order**

#### **Adoption of Agenda**

#### **Minutes of Previous Meetings**

1. Minutes of Previous Council Meeting - July 8, 2014
2. Minutes of Previous Council Meeting - July 9, 2014

#### **Delegations**

*Those individuals in attendance at the meeting will be provided with an opportunity to address Council regarding an item on the agenda, with the exception of those items for which a Public Hearing is required or has been held. Consistent with all delegations, each presentation will be allowed a maximum of five minutes.*

#### **Public Hearings and Related Reports**

3. Closure of Forestry Road – FRD 640002 within the Fort Hills Oil Sands Project Area – Bylaw No. 14/027  
- public hearing

4. Closure of Undeveloped Government Road Allowance – Hamlet of Anzac – Bylaw No. 14/028  
- public hearing
5. Athabasca Power Centre – Area Structure Plan Amendment Bylaw No. 14/029 and Land Use Bylaw Amendment Bylaw No. 14/030  
- public hearing

### **Bylaws**

6. Land Use Bylaw Amendment – Lot 1, Block 9, Plan 132 2526 and a portion of Lot 1, Block 3, Plan 082 4071 (Longboat Landing) Bylaw No. 14/032  
- 1st reading (Proposed Date for public hearing, 2nd and 3rd reading - September 9, 2014)

### **Reports**

7. Council Committee Appointments

### **New and Unfinished Business**

8. Notice of Motion – MacDonald Island Parkade and Transit

### **Reporting - Boards and Committees**

### **Adjournment**

**Unapproved Minutes of a Meeting of the Council of the Regional Municipality of Wood Buffalo held in the Council Chamber at the Municipal Offices in Fort McMurray, Alberta, on Tuesday, July 08, 2014, commencing at 6:00 p.m.**

**Present:** M. Blake, Mayor  
T. Ault, Councillor  
G. Boutilier, Councillor  
L. Bussieres, Councillor  
S. Germain, Councillor  
K. McGrath, Councillor  
P. Meagher, Councillor  
J. Stroud, Councillor  
A. Vinni, Councillor

**Absent:** J. Cardinal, Councillor  
J. Chadi, Councillor

**Administration:** M. Ulliac, Chief Administrative Officer (Interim)  
A. Rogers, Acting Chief Legislative Officer  
A. Hawkins, Legislative Officer  
D. Soucy, Legislative Officer  
J. Wall, Legislative Coordinator

**National Anthem**

**Call To Order**

The Mayor called the meeting to order at 6:08 p.m.

**Opening Prayer**

Mayor Blake invited those so inclined to join her in prayer.

**Adoption of Agenda**

Moved by Councillor P. Meagher that the Agenda be adopted as presented.

CARRIED UNANIMOUSLY

**Notice of Motion**

Councillor T. Ault served notice that the following motion will be brought forward for Council's consideration at the August 26, 2014 Council Meeting:

"THAT Administration, in conjunction with the Regional Recreation Corporation, develop a parking and traffic strategy for MacDonald Island, including but not limited to a parkade, enhanced transit services or other methods of transportation, and;

THAT the cost and funding options for the strategy be presented to the Audit and Budget Committee for consideration as part of the 2015 budget discussions."

## **Minutes of Previous Meetings**

### **1. Minutes from previous Council Meeting - June 24, 2014**

Moved by Councillor J. Stroud that the Minutes from the June 24, 2014 Council Meeting be approved as presented.

CARRIED UNANIMOUSLY

## **Delegations**

### **2. Patrick Duggan, President, Royal Canadian Legion Branch #165 re: funding request**

**Patrick Duggan, President, and Ken McPherson, 1<sup>st</sup> Vice-President, Royal Canadian Legion Branch #165**, spoke to the plan for the renovations of the Legion building and the estimated costs.

Moved by Councillor P. Meagher that the presentation by Patrick Duggan be accepted as information.

CARRIED UNANIMOUSLY

**Shaun Janvier, resident, Conklin**, via teleconference, spoke with concerns regarding Servicing, Funding and Phasing Strategy Selection, Rural Water and Sewer.

**Verna Quintal-Janvier, resident, Conklin**, via teleconference, addressed some concerns, but also spoke in support of Servicing, Funding and Phasing Strategy Selection, Rural Water and Sewer.

Councillor G. Boutilier left the Chamber at 6:38 p.m. and returned at 6:40 p.m.

**Shirley Tremblay, resident, Conklin**, via teleconference, spoke in support of Servicing, Funding and Phasing Strategy Selection, Rural Water and Sewer.

**Yvonne McCallum, resident, Conklin**, via teleconference, spoke in support of Servicing, Funding and Phasing Strategy Selection, Rural Water and Sewer.

**Val Quintal, resident, Conklin**, via teleconference, spoke in support, but addressed concerns of costs of Servicing, Funding and Phasing Strategy Selection, Rural Water and Sewer.

**Margaret A. Quintal, resident, Conklin**, via teleconference, spoke in support of Servicing, Funding and Phasing Strategy Selection, Rural Water and Sewer.

**Constance Scarlett, Executive Director, Arts Council Wood Buffalo**, spoke in support of Public Art Committee Bylaw No. 14/020.

**John Kuerbis, resident, Sapræ Creek**, spoke in support of Servicing, Funding and Phasing Strategy Selection, Rural Water and Sewer, but spoke in opposition to it in Sapræ Creek.

**Rick Mikoniuk, resident, Conklin**, spoke in support of Servicing, Funding and Phasing Strategy Selection, Rural Water and Sewer.



**Peter Allen, resident**, spoke in support of Regional Recreation Corporation - North Side Recreation Centre.

**Jim Rogers, resident**, spoke in support of Servicing, Funding and Phasing Strategy Selection, Rural Water and Sewer. Mr. Rogers also spoke to the Northside Recreation Centre, Public Art Committee Bylaw No. 14/020 and flood mitigation.

Councillor G. Boutilier exited the chamber at 7:21 p.m.

**Gail Philips, resident, Sapræ Creek**, spoke in opposition to Servicing, Funding and Phasing Strategy Selection, Rural Water and Sewer in Sapræ Creek.

**Bob Galbraith, resident, Conklin**, spoke in support of Servicing, Funding and Phasing Strategy Selection, Rural Water and Sewer.

**Jeffery O'Donnell and Ron Quintal, residents, Conklin**, spoke in support of Servicing, Funding and Phasing Strategy Selection, Rural Water and Sewer.

**Elmer Herman and Jules Nokohoo, residents, Janvier**, spoke in support of Servicing, Funding and Phasing Strategy Selection, Rural Water and Sewer.

**Russell Thomas, resident**, spoke in support of Public Art Committee Bylaw No. 14/020.

**Greg Naundorf, resident, Anzac**, spoke in support of Servicing, Funding and Phasing Strategy Selection, Rural Water and Sewer.

**Nick Parkinson, President and Chief Operating Officer, YMCA of Wood Buffalo**, spoke in support of Regional Recreation Corporation - North Side Recreation Centre.

**Shawn Styles and Jack Tobin, residents, Anzac**, spoke in support of Servicing, Funding and Phasing Strategy Selection, Rural Water and Sewer.

**Mike Durocher, resident**, spoke in support of Servicing, Funding and Phasing Strategy Selection, Rural Water and Sewer, as well as Regional Recreation Corporation - North Side Recreation Centre and Public Art Committee Bylaw No. 14/020.

**LeMont Edwards, resident, Gregoire Lake Estates**, spoke regarding Servicing, Funding and Phasing Strategy Selection, Rural Water and Sewer in Gregoire Lake Estates and requested that consideration be given to a full pressure system.

**Paul Hunt, Gene Ouellette and Jack (Torchy) Peden, residents**, spoke in support of the Waterfront Steering Committee - Bylaw No. 14/031.

**Damien Asher, resident, Sapræ Creek**, spoke regarding Servicing, Funding and Phasing Strategy Selection, Rural Water and Sewer, and expressed concerns as to the cost that residents may incur to replace septic systems prior to work for the project being completed.

### **Recess**

A break occurred between 8:33 p.m. and 8:54 p.m.

Councillor G. Boutilier returned to the chamber at 8:54 p.m.

Moved by Councillor T. Ault that all delegations heard this evening be received as information.

CARRIED UNANIMOUSLY

### **Presentations**

#### **3. Regional Recreation Corporation - North Side Recreation Centre**

**Mike Cachia, Chair, Tim Reid, Chief Executive Officer, Regional Recreation Corporation and Michael Roma, RC Strategies** gave an overview of the plans for the Northside Recreation Centre. Discussion ensued regarding the centre and budgetary requirements needed to begin construction of the twin arenas.

### **Extension of Meeting Time**

Moved by Councillor J. Stroud that the meeting be extended past 10:00 p.m.

CARRIED UNANIMOUSLY

Moved by Councillor S. Germain that the 2014 Capital Budget be amended by increasing the funding for the Northside Recreation Centre Twin Arena project by \$13M; and that the funding be allocated from:

- a) the Corporate Operating Surplus
  - b) the Emerging Issues Reserve,
- in that order.

CARRIED UNANIMOUSLY

Moved by Councillor S. Germain that the proposed final design and costing recommendation for the Northside Recreation Centre be accepted as information; and that the Northside Recreation Centre project, in the amount of \$153 million be included in the 2015 Capital Budget for Council's consideration.

CARRIED

For: M. Blake, T. Ault, G. Boutilier, S. Germain, K. McGrath, P. Meagher, J. Stroud, A. Vinni

Opposed: L. Bussieres

### **Recess**

A break occurred between 10:40 p.m. and 10:59 p.m.

Moved by Councillor K. McGrath that Council deal with Item #9 - Royal Canadian Legion #165 Funding Request; and that the meeting be recessed until 5:00 p.m. on Wednesday, July 9, 2014.

CARRIED UNANIMOUSLY

## **Reports**

### **9. Royal Canadian Legion #165 Funding Request**

**Carole Bouchard, Director, Community Services**, gave an overview of the funding request for the Royal Canadian Legion.

Moved by Councillor G. Boutilier:

1. that the Royal Canadian Legion's request for \$495,000 capital grant for facility upgrades be approved; and
2. that the capital grant be funded from:
  - the 2014 Community Services Operating Budget;
  - the Community and Corporate Services Division Operating surplus;
  - the 2014 Corporate Operating Budget surplus; or
  - the Emerging Issues Reserve,in that order.

CARRIED UNANIMOUSLY

## **Recess**

The meeting recessed at 11:23 p.m.

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Mayor

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Chief Legislative Officer



**Unapproved Minutes of a Meeting of the Council of the Regional Municipality of Wood Buffalo held in the Council Chamber at the Municipal Offices in Fort McMurray, Alberta, on Wednesday, July 09, 2014, commencing at 5:00 p.m.**

**Present:** A. Vinni, Deputy Mayor  
T. Ault, Councillor  
G. Boutilier, Councillor  
L. Bussieres, Councillor  
J. Cardinal, Councillor (via teleconference)  
S. Germain, Councillor  
K. McGrath, Councillor  
P. Meagher, Councillor  
J. Stroud, Councillor

**Absent:** M. Blake, Mayor  
J. Chadi, Councillor

**Administration:** M. Ulliac, Interim Chief Administrative Officer  
A. Rogers, Acting Chief Legislative Officer  
A. Hawkins, Legislative Officer  
D. Soucy, Legislative Officer  
S. Harper, Legislative Officer

**Reconvening of the July 8, 2014 Council Meeting**

The Council meeting reconvened at 5:01 p.m. on Wednesday July 9, 2014.

Deputy Mayor A. Vinni gave a brief explanation that the meeting was a continuation of the July 8, 2014 Council meeting which recessed the previous night.

**Bylaws**

**4. Public Art Committee Bylaw No. 14/020 Municipal Public Art Policy PRL – 160**

Moved by Councillor P. Meagher that Bylaw 14/020, being a bylaw to establish the Public Art Committee, be read a second time.

Councillor G. Boutilier exited the meeting at 5:39 p.m. and reentered at 5:41 p.m.

CARRIED

For: A. Vinni, T. Ault, L. Bussieres, J. Cardinal, S. Germain, K. McGrath, P. Meagher, J. Stroud  
Opposed: G. Boutilier

Moved by Councillor T. Ault that Bylaw No. 14/020 be read a third and final time.

CARRIED UNANIMOUSLY

The following motion was presented by Councillor S. Germain:

“That the Municipal Public Art Policy (PRL-16) dated July 9, 2014 be approved.”

Moved by Councillor S. Germain that the Public Art Policy be amended to be reviewed in one year.

CARRIED UNANIMOUSLY

Voting then occurred on the original motion as amended:

Moved by Councillor S. Germain that the Municipal Public Art Policy (PRL-160), dated July 9, 2014, be approved as amended.

CARRIED UNANIMOUSLY

**5. Closure of Forestry Road - FRD 640002 within the Fort Hills Oil Sands Project Area – Bylaw No. 14/027**

Moved by Councillor P. Meagher that Bylaw No. 14/027, being a Bylaw to close Forestry Road – FRD 640002, within the Fort Hills Oil Sands Project area, be read a first time; and that the required public hearing be held on August 26, 2014.

CARRIED UNANIMOUSLY

**Exit**

Councillor T. Ault left the meeting at 6:04 p.m.

**6. Closure of Undeveloped Government Road Allowance – Hamlet of Anzac – Bylaw No. 14/028**

Moved by Councillor K. McGrath that Bylaw No. 14/028 being a bylaw to close an undeveloped government road allowance, be read a first time; and that the required public hearing be held on August 26, 2014.

CARRIED UNANIMOUSLY

**7. Athabasca Power Centre - Area Structure Plan Amendment Bylaw No. 14/029 and Land Use Bylaw Amendment Bylaw No. 14/030**

Moved by Councillor K. McGrath that Bylaw No. 14/029, being the Athabasca Power Centre Area Structure Plan, be read a first time; and that Bylaw No. 14/030, being an amendment to Land Use Bylaw No. 99/059, be read a first time; and that the required public hearing be held on August 26, 2014.

CARRIED

For: A. Vinni, L. Bussieres, J. Cardinal,  
S. Germain, K. McGrath, P. Meagher, J.  
Stroud

Opposed: G. Boutilier

**8. Waterfront Steering Committee - Bylaw No. 14/031**

Moved by Councillor P. Meagher that Bylaw No. 14/031, being a bylaw to establish the Waterfront Steering Committee, be read a first time.

CARRIED UNANIMOUSLY

Moved by Councillor K. McGrath that Bylaw No. 14/031 be read a second time.

CARRIED UNANIMOUSLY

Councillor K. McGrath left the meeting at 6:25 p.m. and reentered at 6:27 p.m.

Following a brief discussion it was agreed that the appointment of the Chair and Vice Chair would follow the standard Selection Committee process.

Moved by Councillor G. Boutilier that Bylaw No. 14/031 be considered for third reading.

CARRIED UNANIMOUSLY

Moved by Councillor P. Meagher that Bylaw No. 14/031 be read a third and final time.

CARRIED UNANIMOUSLY

**Recess**

A break occurred from 6:40 p.m. to 7:01 p.m.

**Reports****10. Flood Mitigation - Reach 1 and Snye Point Development**

Moved by Councillor G. Boutilier that the Flood Mitigation – Reach 1 project proceed as identified in Schedule 1.

A request was made to have administration investigate options to make the berm in Schedule 1 more culturally interesting through the inclusion of waves or ripples rather than the flat-topped proposal.

CARRIED UNANIMOUSLY

Councillor G. Boutilier exited the meeting at 7:50 p.m. and reentered at 7:52 p.m.  
Councillor S. Germain exited the meeting at 8:26 p.m. and reentered at 8:28 p.m.  
Councillor G. Boutilier exited the meeting at 8:30 p.m. and reentered at 8:32 p.m.

**Recess**

A break occurred from 8:57 p.m. to 9:20 p.m.

The following motion was presented by Councillor P. Meagher:

“That the Snye Point Development project proceed as identified in Schedule 2.”

A request was made to have benches installed along the Snye for use by the public by the weekend.

In response to the request, the Director of Engineering indicated that benches could be installed along the Snye for public use, and that staff would endeavor to complete the work by the forthcoming weekend.

Moved by Councillor K. McGrath:

- That Schedule 2 be amended to include only rough grading, dredging and weed harvesting, general park improvements, two new boat launches and connecting roads, amenities building and park lighting; and
- That administration undertake an amendment and addition to the contract to ensure that adequate security exists to mitigate any potential impact or liability to the Municipality and citizens in the event of non-performance by the specified date mitigated by the addition of manpower and equipment to bring the schedule to completion ahead of the Western Canada Summer Games of 2015.

CARRIED UNANIMOUSLY

Voting then occurred on the original motion as amended:

Moved by Councillor P. Meagher that the Snye Point Development project proceeds as identified in Schedule 2 as amended.

CARRIED UNANIMOUSLY

#### **Extension of the Meeting:**

Moved by Councillor P. Meagher that the meeting be extended beyond 10:00 pm.

CARRIED

For: A. Vinni, L. Bussieres, J. Cardinal, S. Germain, K. McGrath, P. Meagher, J. Stroud

Opposed: G. Boutilier

#### **11. 2014 and Prior Capital Budget Amendments - Revised and New Projects**

Moved by Councillor P. Meagher:

- That the 2014 and Prior Capital Budget be revised as summarized on Attachment 1 (2014 and Prior Capital Budget Amendment – Revised and New Projects), dated July 8, 2014; and
- That the cash flow of Capital Projects be approved as summarized on Attachment 2 (2014 and Prior Capital Budget Amendment – Project Cash Flow Summary), dated July 8, 2014.

CARRIED UNANIMOUSLY



Councillor G. Boutilier left the meeting at 10:02 p.m. and reentered at 10:03 p.m.

## **12. Servicing, Funding and Phasing Strategy Selection, Rural Water and Sewer**

Kevin Scoble, Acting Executive Director of Public Infrastructure and Planning, made a presentation on the Servicing, Funding, and Phasing Strategy Selection for Rural Water and Sewer services.

Councillor S. Germain left the meeting at 10:27 p.m. and reentered at 10:29 p.m.

Moved by Councillor J. Stroud that the Municipality provide the following water and sewer systems:

- Full pressure water/gravity sewer to the communities of Anzac, Conklin and Gregoire Lake Estates based on growth projections and development strategies as outlined in the Municipal Development Plan and Area Structure Plans; and
- Trickle fill water/low pressure sewer systems to the communities of Draper and Janvier; and
- Low pressure sewer to the community of Sapræ Creek.

CARRIED UNANIMOUSLY

Councillor G. Boutilier left the meeting at 11:13 p.m. and reentered at 11:14 p.m.

Moved by Councillor J. Stroud:

- That Administration proceed with detailed engineering design of the community-specific systems and the respective grant applications; and
- That individual property owners in the above communities be assessed a fee of 10% of the total projects costs through a Local Improvement Program, one-time lump sum payment or other financing options. Further, that administration provide recommendations to Council by September 23, 2014 on available financing options to provide flexibility to individual property owners in paying the fee; and
- That a capital budget request for Rural Water and Sewer Construction be submitted for consideration as part of the 2015 Capital Budget deliberations.

CARRIED UNANIMOUSLY

## **New and Unfinished Business**

### **13. Notice of Motion – Independent Work Camps**

Moved by Councillor K. McGrath that Administration be directed to investigate the available options for obtaining greater revenue for the municipal treasury from open or independent work camps [i.e., camps not directly owned by or affiliated with an oil sands company] that operate within the boundaries of the Municipality, in order to more equitably compensate for increased costs to the Municipality that arise from the operation of such camps, and that Administration report back to Council on this matter after the 2014 summer recess.

CARRIED UNANIMOUSLY

## **Reporting - Boards and Committees**

Councillor S. Germain – Strategic Planning process; Accountability, Integrity and Transparency Audit.

Councillor K. McGrath – Commended Council and Administration for their hard work and efforts in providing services to the citizens of the region.

Councillor J. Stroud – Conklin Community Graduation Celebration; Veterans Memorial Highway Association.

## **Adjournment**

Moved by Councillor P. Meagher that the meeting be adjourned.  
CARRIED UNANIMOUSLY

The meeting adjourned at 11:46 p.m.

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Mayor

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Chief Legislative Officer

**3. Public Hearing re: Closure of Forestry Road – FRD 640002 within the Fort Hills Oil Sands Project Area – Bylaw No. 14/027**

- A. Introduction from Administration
  - Keith Smith, Acting Director of Land Administration
- B. Opening Statement from Applicant
- C. Written Presentations
  - none received
- D. Verbal Presentations
  - none received
- E. Other Verbal Presentations (Time Permitting and with Consent of Council)
- F. Questions of Council
- G. Closing Statement from Applicant
- H. Closing Statement from Administration



**Subject: Closure of Forestry Road – FRD 640002 within the Fort Hills Oil Sands Project Area – Bylaw No. 14/027****APPROVALS:**

Keith Smith, Acting Director  
Brian Makey, Chief Operating Officer  
Marcel Ulliach, Interim Chief Administrative Officer

**Process Note:**

Following the Public Hearing being held, Bylaw No. 14/027 will be submitted for signature to the Minister of Transportation. Once Bylaw No. 14/027 is signed by the Minister it will be scheduled for consideration of second and third readings.

**Summary:**

Suncor Energy Inc. has requested that a forestry road, FRD 640002, located within the Fort Hills Oil Sands Project area (as highlighted in red on Attachment 2) be closed, in order to facilitate development of this project and address safety concerns related to public access within the mining operations area.

**Background:**

Forestry road, FRD 640002 is located within the Fort Hills Oil Sand Project area, approximately 80 km north of Fort McMurray within Townships 96 and 97, Range 10, W4M. Suncor Energy Inc.'s Fort Hills project area is located on Crown land and is held under leases with the Province, as well as the Alberta Energy Regulator.

The Municipality has direction, control and management of FRD 640002 while ownership resides with the Crown. FRD 640002 was originally constructed by the Department of Lands and Forests to provide access to a fire lookout tower. Jurisdiction over all forestry roads within the Municipality was transferred from Alberta Transportation to the Municipality under Section 9 of a Transportation Transitional Funding Agreement, adopted by Council on March 26, 1996, by Resolution No. 96/206.

In accordance with the Municipal Government Act (MGA), a road closure must be effected by a bylaw, which requires Council approval. The bylaw must be advertised and a public hearing must also be held, after the bylaw is given first reading, in order to provide an opportunity for individuals who may be affected by the road closure to be heard by Council. The bylaw has no effect unless approval is given by the Minister of Transportation (AT) prior to the bylaw being given second reading. In addition to the requirements under the MGA, notification of the road closure bylaw to Alberta Environment and Sustainable Resource Development (AESRD) must be undertaken after completion of the bylaw process in order to cancel the forestry road disposition held with the Province.

The proposed road closure has been circulated to internal municipal departments, AT, AESRD as well as franchise and utility companies. No issues or concerns were raised.

Under the MGA, former forestry roads are open to public travel. A municipality may enter into an agreement with a person with respect to a former forestry road that authorizes the person to use the road for commercial and industrial purposes. Entering into an agreement of this type would pose a public safety concern, as the roadway runs directly through what will eventually be an active mining operation. However, if the proposed road closure is approved, Suncor will legally control and manage access through the Fort Hills project area, which has an estimated life span of 40 years. Alternate and existing public roadways will provide access to and around the site. Access to the bitumen fire tower is no longer required as the site has been decommissioned. A water testing site managed by AESRD is also located along this roadway however AESRD has confirmed that closure of the road will not conflict with access to the testing site.

**Budget/Financial Implications:**

There will be a minimal financial impact to the Municipality if the proposed road closure is approved as the Municipality currently charges a fee of \$1,000 per year to industrial companies for use of this road. However, this is an administrative fee only and does not provide any real financial benefit. Once the road is closed the land will be transferred to the Province for administration.

**Rationale for Recommendations:**

Administration supports the road closure because the road is no longer required for its intended purpose, and closure will facilitate development of the Fort Hills Oil Sands Project. Further, public safety concerns related to traffic management issues through the mine site will be addressed by diverting vehicle traffic through alternate routes around the site and away from the mining operations.

**Attachments:**

1. Bylaw No. 14/027
2. Subject Area Map

**BYLAW NO. 14/027**

**BEING A BYLAW OF THE REGIONAL MUNICIPALITY OF WOOD BUFFALO TO  
CLOSE A FORESTRY ROAD**

**WHEREAS** the lands hereafter described are no longer required for public travel; and

**WHEREAS** application has been made to Council to have the road closed; and

**WHEREAS** the Council of the Regional Municipality of Wood Buffalo deems it expedient to provide for a bylaw for the purpose of closing to public travel of certain roads, or portions thereof, situated in the said municipality, and therefore disposing of same; and

**WHEREAS** notice of the intention of Council to pass a bylaw has been given in accordance with Section 606 of the Municipal Government Act; and

**WHEREAS** Council was not petitioned for an opportunity to be heard by any person claiming to be prejudicially affected by the bylaw.

**NOW THEREFORE BE IT RESOLVED** that the Council of the Regional Municipality of Wood Buffalo in the Province of Alberta does hereby close to public travel for the purpose cancelling the disposition and transferring the lands back to Environment and Sustainable and Resource Development and returning of the following, subject to rights of access granted by other legislation:

1. All that portion of FRD640002 located within Meridian 4, Range 10, Townships 96 and 97

**EXCEPTING THEREOUT ALL MINES AND MINERALS**

2. This Bylaw comes into force when it is passed.

READ a first time this 9<sup>th</sup> day of July, 2014.

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Mayor

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Chief Legislative Officer

APPROVED this \_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_ .

\_\_\_\_\_  
Minister of Transportation

READ a second time this \_\_\_\_\_ day of \_\_\_\_\_, 2014.

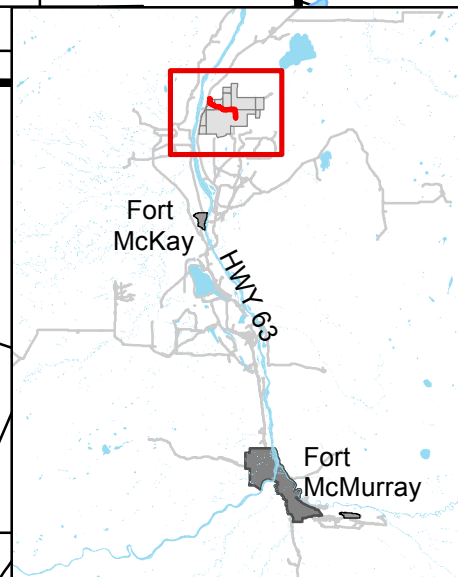
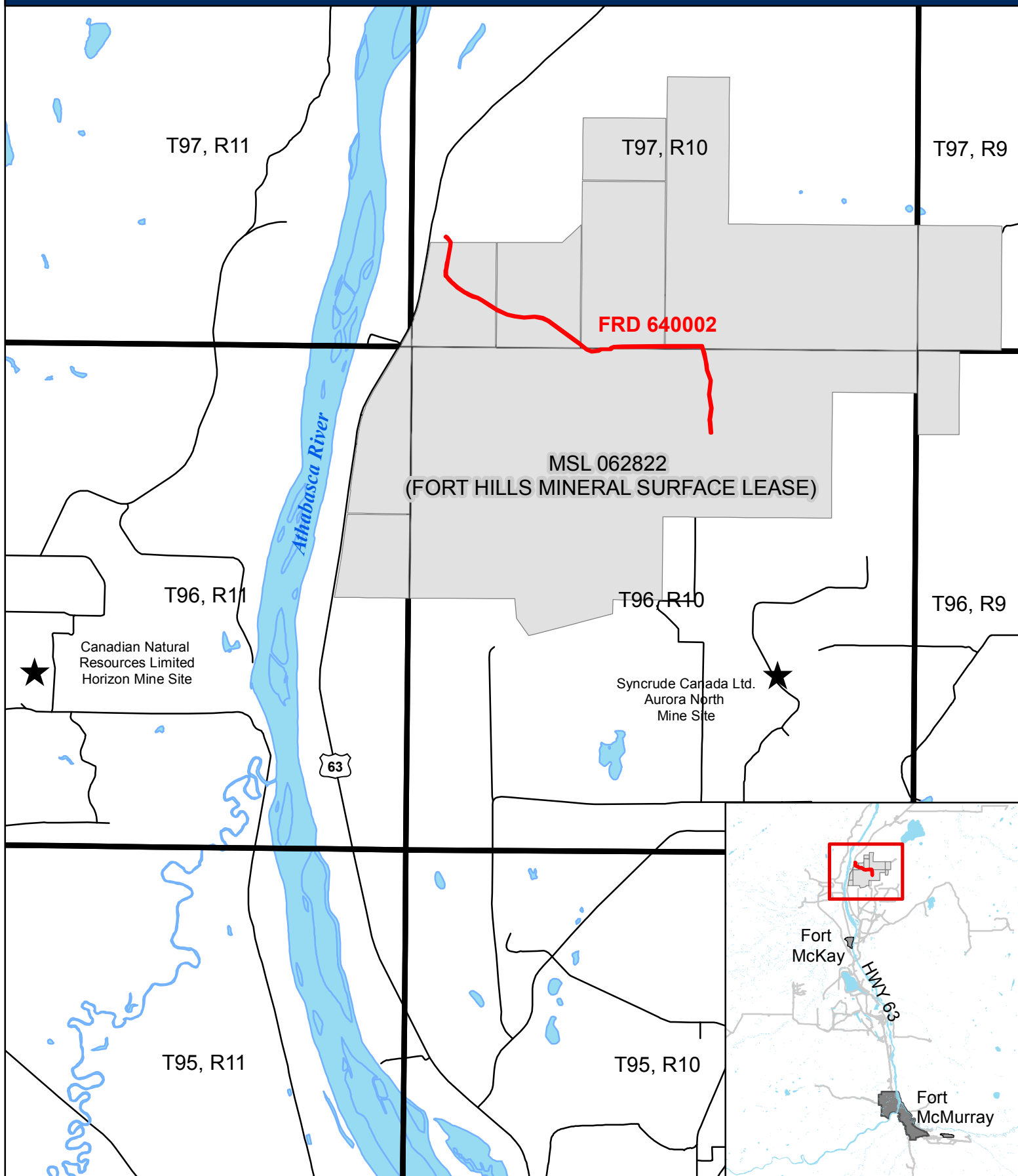
READ a third and final time this \_\_\_\_\_ day of \_\_\_\_\_, 2014.

SIGNED and PASSED this \_\_\_\_\_ day of \_\_\_\_\_, 2014.

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Chief Legislative Officer





FRD 640002

MSL 062822

0 1.25 2.5 5 Kilometers



**ROAD CLOSURE**  
**FRD 640002**  
**TWP 96-97, RGE 10, W4M**



REGIONAL MUNICIPALITY  
 OF WOOD BUFFALO

Map created by the Land Administration Department  
 June, 2014  
 BA006



**4. Public Hearing re: Closure of Undeveloped Government Road Allowance – Hamlet of Anzac – Bylaw No. 14/028**

- A. Introduction from Administration and Opening Statement
  - Keith Smith, Acting Director of Land Administration
- B. Written Presentations
  - none received
- C. Verbal Presentations
  - none received
- D. Other Verbal Presentations (Time Permitting and with Consent of Council)
- E. Questions of Council
- F. Closing Statement from Applicant
- G. Closing Statement from Administration



**Subject: Closure of Undeveloped Government Road Allowance –  
Hamlet of Anzac – Bylaw No. 14/028****APPROVALS:**

Keith Smith, Acting Director  
Brian Makey, Chief Operating Officer  
Marcel Ulliac, Interim Chief Administrative Officer

**Process Note:**

Following the Public Hearing being held, Bylaw No. 14/028 will be submitted for signature to the Minister of Transportation. Once Bylaw No. 14/028 is signed by the Minister it will be scheduled for consideration of second and third readings.

**Summary:**

A portion of an undeveloped government road allowance located east of SE 8-86-7-4 (as highlighted in red on Attachment 2) and within the proposed Phase One residential lands (as highlighted in yellow on Attachment 2), in the Hamlet of Anzac requires closure in order to enable the consolidation of the road allowance with the adjacent municipal properties. Closure and consolidation will allow for the creation of proposed Phase One residential land being approximately 52 acres in size.

**Background:**

The Hamlet of Anzac is designated as the rural growth area in the Municipal Development Plan. In 2012 Council approved an Area Structure Plan to guide the development of the Hamlet. The subject road allowance is within a 52 acre parcel which has been designated as the Phase One lands for residential development.

To consolidate the undeveloped government road allowances with adjacent properties, the road allowance must be closed by bylaw. A public hearing must also be held after the bylaw is given a first reading in order to provide an opportunity for any individuals who may be affected by the closure, to be heard by Council. A bylaw closing a road allowance in the Rural Service Area has no effect unless it is approved by the Minister of Transportation before the bylaw receives a second reading.

The portion of road allowance designated for closure has an area totaling approximately 0.63 Ha (1.55 Ac).

The proposed road allowance closure was circulated to internal Municipal departments, Alberta Transportation, Alberta Environment and Sustainable Resource Development, along with franchise and utility companies. No objections were raised regarding the proposal, and the road allowance closure will have no impact on Municipal operations. The Road Closure Bylaw has been reviewed by Alberta Land Titles to ensure the accuracy of the legal descriptions.

**Budget/Financial Implications**

No financial impact to the Municipality will result from closure of the road allowance area as the allowance is undeveloped, and does not contain a roadway.

An appraisal to determine current market value for the portion of undeveloped government road allowance to be closed was not undertaken because the lands are to be consolidated with adjacent properties owned by the Municipality, not a third party.

**Rationale for Recommendations:**

Administration supports the road allowance closure and the consolidation with adjacent municipal properties because the road allowance is undeveloped and does not provide physical access to public utilities nor does it serve as public access to any of the adjacent lands. Further, there will be a new configuration of roads established as part of the subdivision development to provide access in the residential subdivision.

**Attachments:**

1. Proposed Bylaw No. 14/028
2. Subject Area Map

**BYLAW NO. 14/028**

**BEING A BYLAW OF THE REGIONAL MUNICIPALITY OF WOOD BUFFALO FOR THE PURPOSE OF CLOSING AND CREATING TITLE TO UNDEVELOPED GOVERNMENT ROAD ALLOWANCE**

**WHEREAS** application has been made to the Council of the Regional Municipality of Wood Buffalo to close an undeveloped government road allowance pursuant to the requirements of section 22 of the Municipal Government Act, c.M-26, RSA 2000, as amended;

**WHEREAS** the Council of the Regional Municipality of Wood Buffalo deems it expedient to provide for the purpose of closing certain roads, or portions thereof, situated in the said municipality, and thereafter disposing of the same;

**WHEREAS**, notice of the intention of Council to pass a bylaw has been given in accordance with section 606 of the Municipal Government Act;

**WHEREAS**, Council was not petitioned for an opportunity to be heard by an persons claiming to be prejudicially affected by the bylaw; and

**NOW THEREFORE BE IT RESOLVED** that the Council of the Regional Municipality of Wood Buffalo in the Province of Alberta hereby enacts as follows:

1. The following described road allowance is hereby closed for the purpose of creating title subject to the right of access granted by other legislation:

**TITLE NUMBER 1**

FIRST:

MERIDIAN 4 RANGE 7 TOWNSHIP 86

ALL THAT PORTION OF ORIGINAL GOVERNMENT ROAD ALLOWANCE LYING WITHIN PLAN\_\_\_\_\_.

EXCEPTING THEREOUT ALL MINES AND MINERALS.

2. This Bylaw shall become effective when it has passed.

READ a first time this 9<sup>th</sup> day of July, 2014.

---

Mayor

---

Chief Legislative Officer

APPROVED this \_\_\_\_ day of \_\_\_\_\_, 20\_\_\_\_.

\_\_\_\_\_  
Minister of Transportation

READ a second time this \_\_\_\_\_ day of \_\_\_\_\_, 2014.

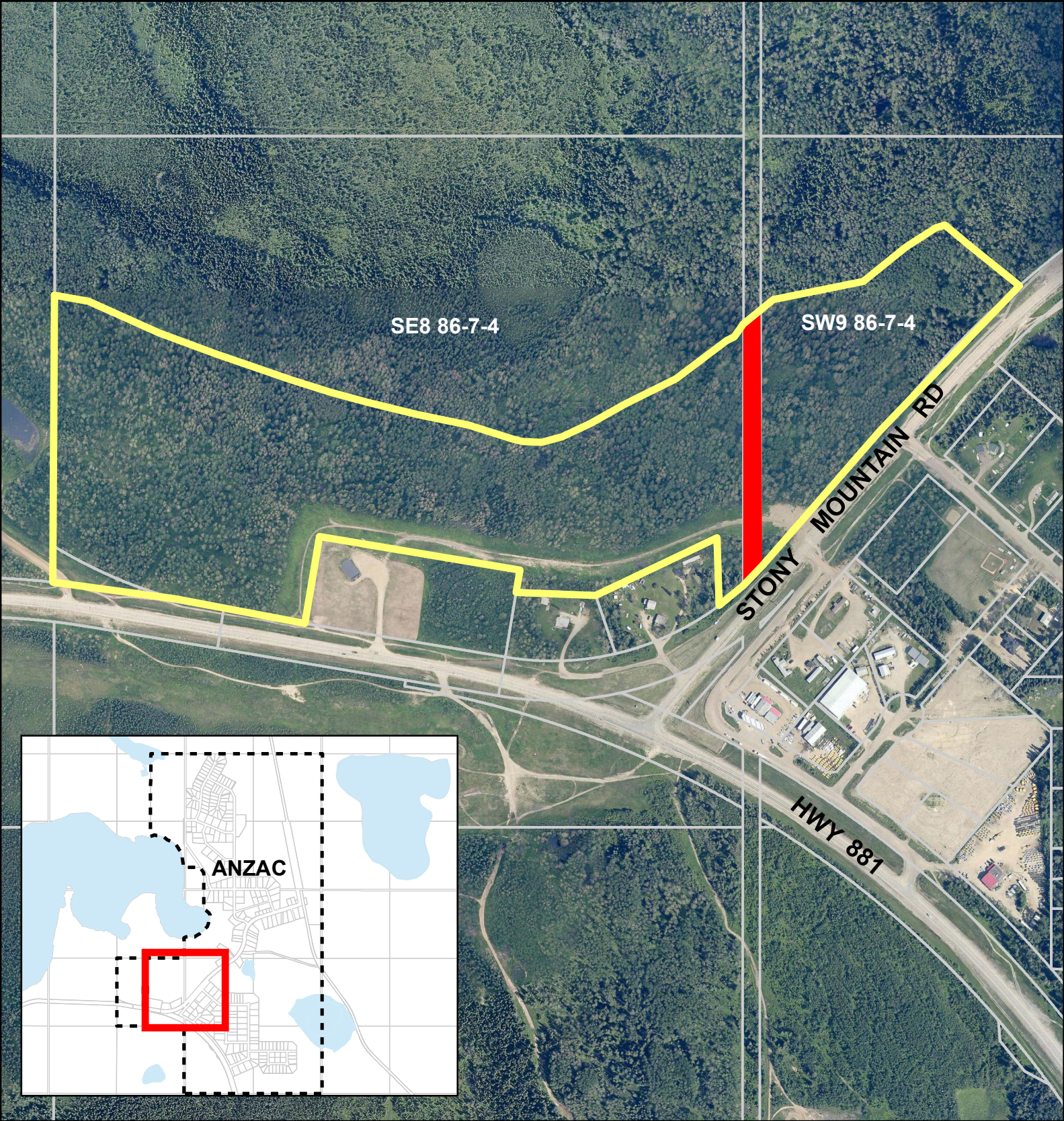
READ a third and final time this \_\_\_\_\_ day of \_\_\_\_\_, 2014.

SIGNED and PASSED this \_\_\_\_\_ day of \_\_\_\_\_, 2014.

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Chief Legislative Officer





- Undeveloped Road Allowance Closure Area
- Proposed Subdivision - Phase 1 of ASP
- Survey Parcels

0 40 80 160 240  
Meters



Portion of SW9 86-7-4



REGIONAL MUNICIPALITY  
OF WOOD BUFFALO

Map created by the Land Administration Department  
April, 2014





**5. Public Hearing re: Athabasca Power Centre – Area Structure Plan  
Amendment Bylaw No. 14/029 and Land Use Bylaw Amendment Bylaw No.  
14/030**

- A. Introduction from Administration
  - Brad Evanson, Manager, Community Development Planning
  - Chris Booth, Planner, Comprehensive Planning
  - Jeff Penney, Manager, Economic Development
- B. Opening Statement from Applicant
- C. Written Presentations
  - none received
- D. Verbal Presentations
  - none received
- E. Other Verbal Presentations (Time Permitting and with Consent of Council)
- F. Questions of Council
- G. Closing Statement from Applicant
- H. Closing Statement from Administration



**Subject: Athabasca Power Centre – Area Structure Plan Amendment  
Bylaw No. 14/029 and Land Use Bylaw Amendment Bylaw  
No. 14/030****APPROVALS:**

Bradley Evanson, Acting Director  
Kevin Scoble, Acting Executive Director  
Marcel Ulliach, Interim Chief Administrative Officer

**Administrative Recommendation:**

THAT consideration of second and third reading of Bylaw No. 14/029 and Bylaw No. 14/030 be deferred to October 14, 2014.

**Summary:**

The Regional Municipality of Wood Buffalo has submitted an application to adopt the Athabasca Power Centre Area Structure Plan (ASP) on a portion of land along the Highway 63 corridor north of Fort McMurray. This ASP is intended to guide the development of a large-format retail shopping centre. To further facilitate the development of such a shopping centre, the Municipality has also proposed to re-designate the same area Shopping Centre Commercial District (C3) from its current Parks and Recreation District (PR) designation.

The authority to amend the Land Use Bylaw and adopt or repeal area structure plans is vested with Council under the *Municipal Government Act*.

**Background:**

The subject lands are currently owned by the Province of Alberta and are anticipated to be sold in the near future. Given the desire for more retail opportunities locally and to facilitate the sale of the lands by the Province, the Municipality has created an ASP to ensure that the lands function as an attractive and orderly developed major retail destination.

**Rationale for Recommendations:**

In 2009, two newly completed studies, the Commercial and Industrial Land Use Study and Business Inventory and Gap Analysis, identified a significant shortage of commercial land and businesses in the region. This shortage was confirmed two years later by residents during public engagement for the Municipal Development Plan (MDP), where lack of retail opportunities was found to be the most common concern. The MDP, adopted in October 2011, addressed this concern by identifying “New Employment Lands” throughout the region, including the subject lands. Consisting of 29.44 hectares (about 73 acres), the subject lands present a timely opportunity to satisfy demand for large-format retail and assist in making Fort McMurray a more desirable place to live and work.

The current interest in the subject lands commenced in earnest at a meeting in Edmonton on February 14, 2014, conducted by representatives from Alberta Infrastructure, Alberta Transportation, and Alberta Environment and Sustainable Resource Development. From the Municipality, representatives from Planning and Engineering were in attendance. During this meeting, several topics related to the sale and development of the subject lands were discussed, including a preliminary sale target date by the Province of April 2014. Other issues with the site that were discussed include flood hazard mitigation, drainage, utility servicing, and access, the latter being of prime interest given the construction of the Parsons Creek Interchange as the primary access infrastructure to and from the site. With this information in hand and the Province's desired timeline for sales activity, Planning staff commenced the accelerated preparation of the Area Structure Plan to guide potential buyers and developers on the buildout of the site. Beginning on March 27, 2014, Municipal staff led a three-day design charrette and open house where an outline for the ASP was created. The charrette was attended by a large number of interested residents and developers, who each made valuable contributions to the plan.

The adoption of a new ASP to guide the development of the subject lands is critical to ensure that they are developed in a manner which satisfies the region's need for retailers – in particular, large format or “big box” businesses. The proposed Athabasca Power Centre ASP will provide more effective guidance than the current Highway 63 North ASP since the latter's policies are highly generalized and would not necessarily result in the form of development being sought. Given that it was adopted in 1999 and development along the highway corridor has changed in recent years, this is an appropriate time to repeal the ASP and begin replacing it with newer statutory documents, beginning with the Athabasca Power Centre ASP. Administration will determine the need for further replacement ASPs covering remaining lands in the corridor.

To more effectively guarantee that the subject lands are developed as a commercial shopping centre, Administration recommends that the Shopping Centre Commercial District (C3) be applied to the site. This district allows for a variety of uses consistent with typical large-format retail developments, including small and large retail stores, restaurants and entertainment venues. Since the Land Use Bylaw is currently being re-written, Planning and Development will further identify appropriate land uses, site provisions, and design guidelines to ensure the objectives identified in the Athabasca Power Centre Area Structure Plan are met.

There are a few external activities and developments that have bearing on the development of a destination, large-format retail power centre at the subject lands. First, the construction of the Parsons Creek Interchange has commenced. This interchange is required to provide secondary access to and from the Parsons Creek development north of Timberlea. Alberta Transportation is constructing the interchange in phases, to provide phased support for development in Parsons Creek. The ultimate buildout (Phase 2) of the interchange will provide an adequate configuration to provide safe and efficient access to and from the subject lands. The preliminary Phase 1 configuration of the interchange that is actively under construction does not appear to provide the same level of access as the ultimate configuration.

A Traffic Impact Assessment prepared by Stantec in January 2013 acknowledges this and recommends that the Phase 2 interchange improvements be developed to facilitate access to and from the subject lands.

Alberta Transportation is expecting Phase 1 of the Parsons Creek Interchange to be open and functional by October 2015, and fully completed (landscaping, etc.) by Spring of 2016. They have not identified timing for Phase 2 of the interchange, as the primary need for the interchange is tied to development activity in Parsons Creek. As such, RMWB staff are in discussions with the Province to establish either a firm timeline on construction of improvements to the interchange to support access to and from the subject lands, or to provide clarity as to a buyer's/developer's responsibility to construct such improvements at their cost.

The second “external” issue is the “land for infrastructure” Transportation Master Agreement entered into by the Province and the Regional Municipality. The details of this agreement impact lands and infrastructure across the Municipality, but in this case there is one element that is directly related to the sale of the subject lands for a regional, large-format destination retail centre. This is the transfer of the 52 acre Town Centre site within Phase 2 of Parsons Creek from Alberta Infrastructure to the Regional Municipality. The Town Centre land use district (Land Use Bylaw Part 8, Sec. 8.4.7) neither includes nor excludes large-format retail uses, whereas the Power Centre would be specifically planned for regional, large-format retail uses.

Furthermore, given that the Town Centre site does not have the prominent visibility of the Power Centre site, and that the infrastructure needed to support “regional” access to the Parsons Creek Town Centre site is being developed in phases on an indeterminate timeline, RMWB staff are of the opinion that the Power Centre site is more suited to being a regional, large-format destination centre than the Town Centre site is, particularly on the accelerated timelines originally proposed by Alberta Infrastructure. Despite this, Provincial staff have expressed concern about whether the two sites would be in competition if they were on the market in the same timeframe, and appear to be prioritizing the Parsons Creek Town Centre site due to its inclusion in the Transportation Master Agreement. Given this, as well as the previously noted target sale date that has expired, RMWB staff are in discussions with Alberta Infrastructure to clarify their position on the Power Centre development vis a vis the Town Centre site. Staff are also attempting to clarify the Province's intended sale date for the Power Centre lands.

Given these two key issues, Administration supports conducting the Public Hearing for Bylaw No. 14/029 and No. 14/030, and then deferring second and third readings to October 14, 2014, to allow staff time to finalize discussions with the Province on these issues.

**Attachments:**

1. Bylaw No. 14/029 – Athabasca Power Centre Area Structure Plan
2. Bylaw No. 14/030 – Land Use Bylaw Amendment





**BYLAW NO. 14/029**

**BEING A BYLAW OF THE REGIONAL MUNICIPALITY OF WOOD BUFFALO TO  
ADOPT THE ATHABASCA POWER CENTRE AREA STRUCTURE PLAN**

**WHEREAS** Section 633 of the *Municipal Government Act*, R.S.A., 2000, c.M-26 and amendments thereto authorizes Council to enact a bylaw adopting An Area Structure Plan.

**NOW THEREFORE**, the Council of the Regional Municipality of Wood Buffalo, in the Province of Alberta, in open meeting hereby enacts as follows:

1. Bylaw No. 14/029, being the Athabasca Power Centre Area Structure Plan as set out in Schedule A, is hereby adopted.
2. Bylaw 99/037 and all amendments thereto is hereby repealed.
3. This bylaw shall be passed and become effective when it receives third reading and is signed by the Mayor and Chief Legislative Officer.

READ a first time this 9<sup>th</sup> day of July, A.D 2014.

READ a second time this \_\_\_\_\_ day of \_\_\_\_\_, A.D. 2014.

READ a third and final time this \_\_\_\_\_ day of \_\_\_\_\_, A.D. 2014.

SIGNED and PASSED this \_\_\_\_\_ day of \_\_\_\_\_, A.D. 2014.

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Chief Legislative Officer





# ATHABASCA POWER CENTRE AREA STRUCTURE PLAN

PREPARED BY THE REGIONAL MUNICIPALITY OF WOOD BUFFALO





## ACKNOWLEDGEMENTS

The Athabasca Power Centre Area Structure Plan (ASP) is the result of collaboration between residents, the Province of Alberta, Industry, CitySpaces Consulting, and the Regional Municipality of Wood Buffalo (the "Municipality"). The Municipality thanks all participants for sharing their time and providing valuable input in shaping the Plan.

The Municipality also thanks those who participated in the design charrette process. Participants include staff from the following Departments and Branches: Community Services, Culture, Economic Development, Emergency Management, Engineering, Parks, Public Operations, and Planning and Development. External participants include Alberta Infrastructure, Alberta Transportation, HDR Corporation, and CitySpaces Consulting.

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Looking northeast towards the Athabasca River from the southern boundary of the ASP area.



# PART 1: SETTING THE STAGE

## 1.0 OVERVIEW

The Athabasca Power Centre Area Structure Plan (ASP) area is located within the Regional Municipality of Wood Buffalo (see Figure 1). The Regional Municipality of Wood Buffalo (the “Municipality”) is among Canada’s largest and fastest growing municipalities, with a population that has doubled since the year 2000. Fuelled by the growth of the Athabasca Oil Sands, the Municipality has struggled to provide enough commercial land to remain a competitive and desirable place to live and do business.

According to the 2012 municipal census, the Municipality serves a total population of 116,407. This includes 72,944 residents living in Fort McMurray, 4,192 residents living in rural hamlets, and 39,271 people living in project accommodations (work camps). There are six First Nations within the Municipality, with a registered population of about 6,200 members, including approximately 2,060 living on reserve. The 2011 Municipal Development Plan (MDP) projects as many as 231,000 people living in the Municipality by 2030. The Municipality aims to direct much of this growth to the Fort McMurray area.

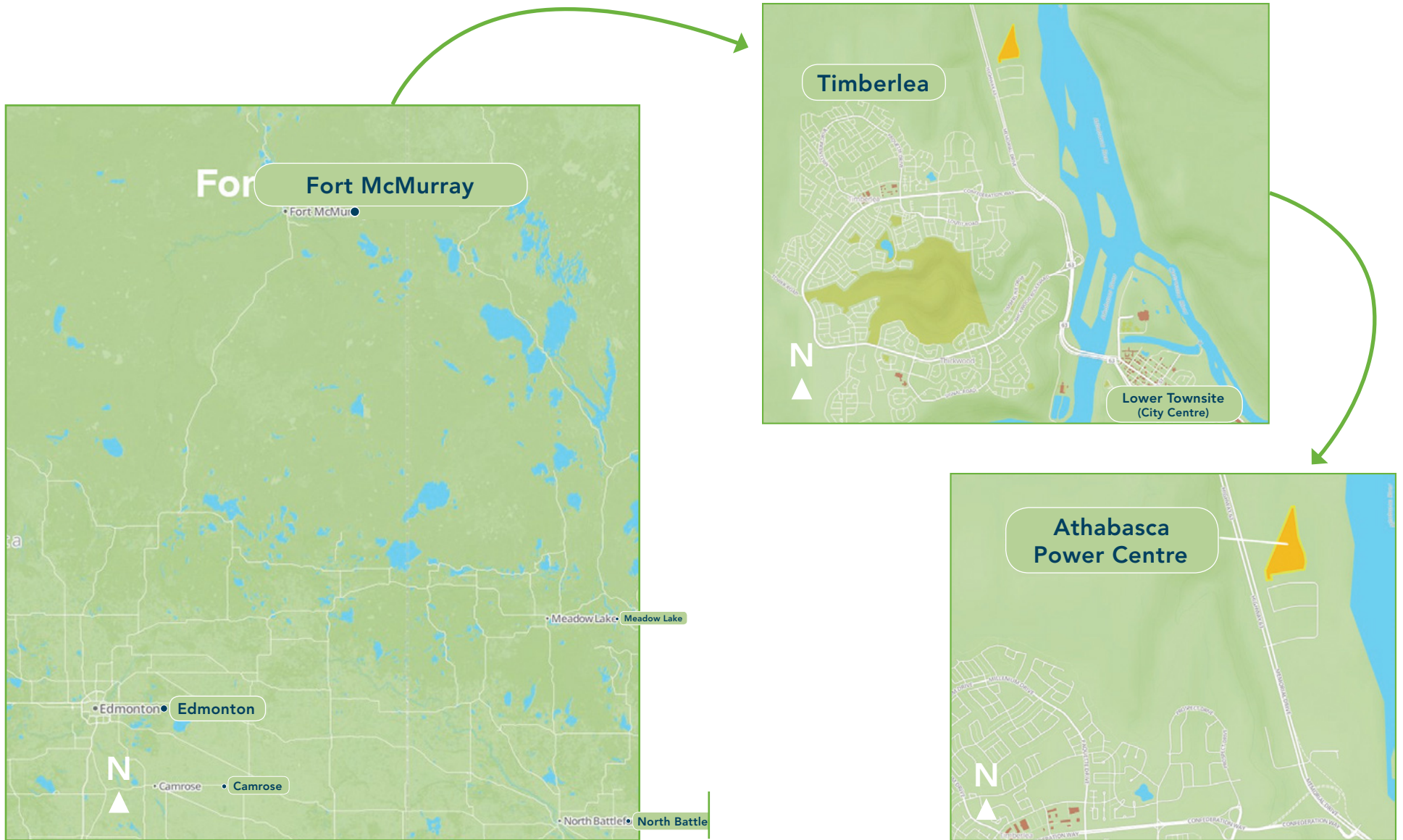
A needs assessment of commercial land reports a clear shortage of retail/commercial land. In 2010 the Commercial and Industrial Land Use Study (CILUS) reported that the Municipality was only providing 2.44 square metres of commercial space per resident, while communities with similar population sizes typically provide closer to 4.18 square metres per resident. CILUS estimated that, assuming future commercial development is constructed at a conventional lot coverage factor of 25.0%, the Municipality would require an additional 159 to 284 hectares (393 to 702 acres) of commercial land by 2030. The Athabasca Power Centre is expected to help address the shortage.

## 1.1 PURPOSE

This Area Structure Plan provides a general development concept for the Athabasca Power Centre site by setting out land uses, transportation connections, infrastructure, and public space. Maps and sketches in this Area Structure Plan are conceptual and are meant to provide general intentions through land use descriptions and approximate locations. Site specific characteristics will be identified through the submission of an Outline Plan.

The specific objectives of the Athabasca Power Centre ASP are to:

- Develop a general layout of the commercial development;
- Create a balanced approach to policy development that enables a range of development possibilities;
- Identify development considerations such as site access, drainage, flood risk, pipeline right-of-ways, and environmental and geotechnical considerations;
- Minimize impact on riparian areas and fish habitat;
- Review servicing constraints;
- Consider site access and layout within the context of future transportation, servicing and other infrastructure; and
- Consider community amenities as well as trail connectivity.



**FIGURE 1: LOCATION MAP OF ATHABASCA POWER CENTRE**



## 1.2 LAND USE CONCEPT

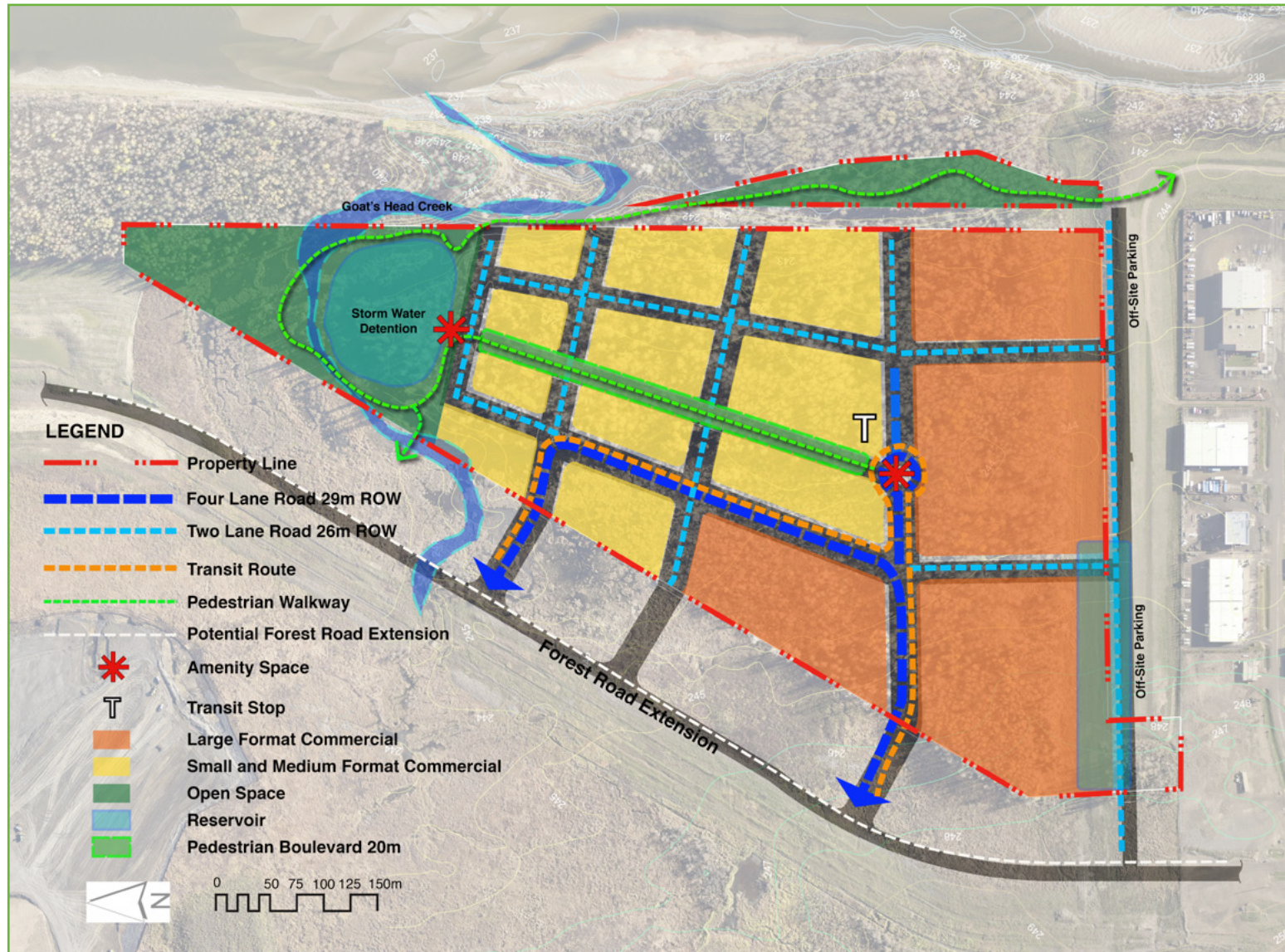
The Athabasca Power Centre ASP will guide the development of a new commercial area that includes large format retail as shown in Figure 3. Figure 3, which provides a conceptual framework for the development of the site, was produced during an intensive three day design charrette involving collaboration between the Municipality, CitySpaces Consulting, industry and the public. Comments and feedback expressed by these stakeholders have helped shape many of the policies in the ASP.

The ASP also considers current and anticipated commercial demand in Fort McMurray and surrounding areas by contemplating an intense layout that accomodates a variety of retail sizes.



FIGURE 2: ATHABASCA POWER CENTRE CHARRETTE, MARCH 2014

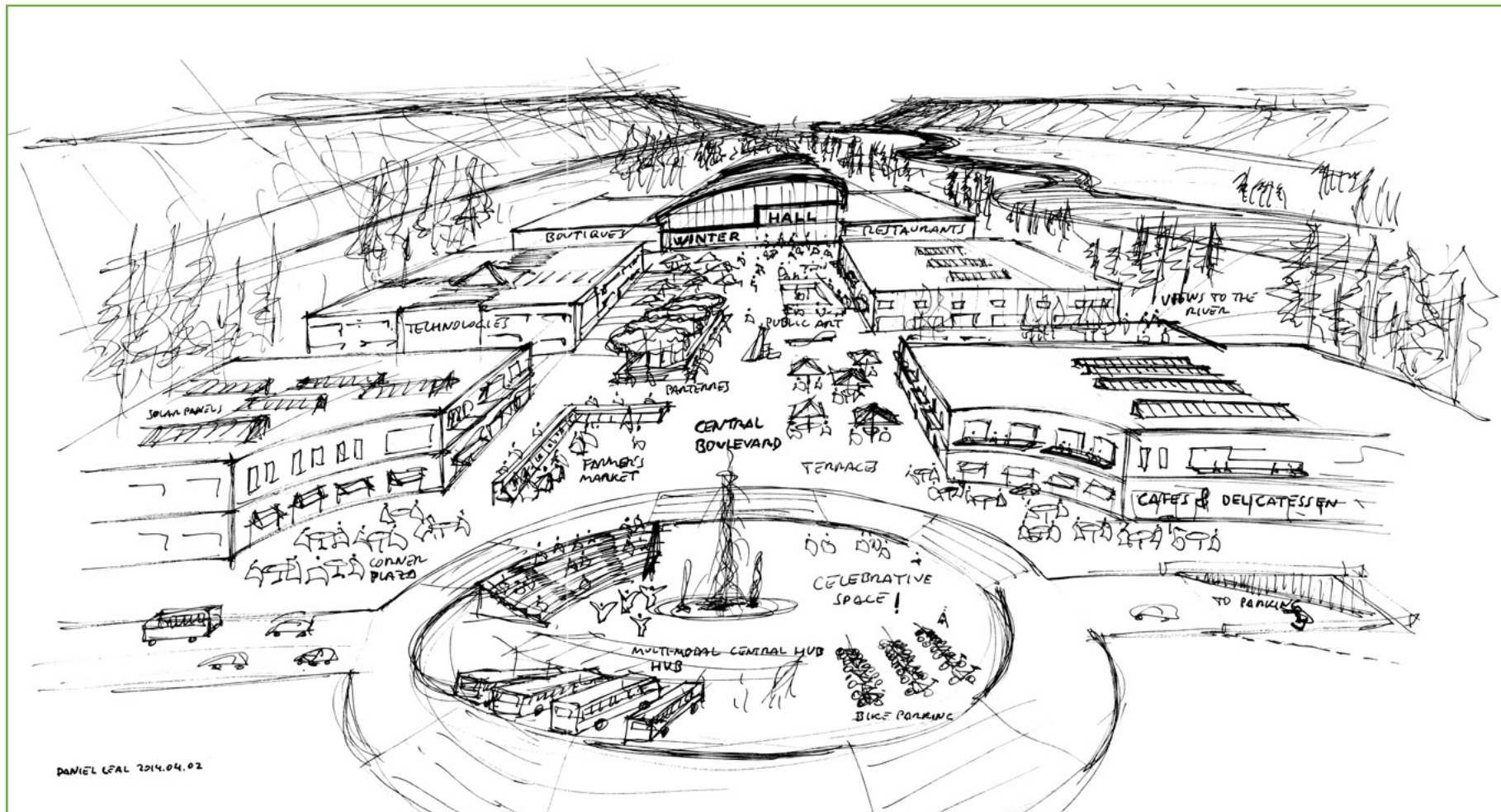




**FIGURE 3: CONCEPTUAL SITE PLAN FOR THE ATHABASCA POWER CENTRE SITE**

Note: For illustrative purposes. Exact concept to be determined at the Outline Plan stage.





**FIGURE 4: PEDESTRIAN BOULEVARD CONCEPT, SKETCH BY DANIEL LEAL**

This conceptual drawing illustrates design elements shown in the Conceptual Site Plan. A key feature is a prominent pedestrian boulevard anchored at either end by a large amenity space. The amenity space at the forefront provides connections to a centrally located public transit stop.

Note: For illustrative purposes. Final site design to be determined at the Outline Plan stage.

### 1.3 DEVELOPMENT OBJECTIVES

The Athabasca Power Centre ASP has been prepared as a comprehensively planned commercial area taking advantage of its proximity to transportation corridors, natural topography, and water features. The main objectives of this ASP are to:

- Develop a plan consistent with the general intent and policies of the Regional Municipality of Wood Buffalo Municipal Development Plan (MDP);
- Provide a framework to aid in the development of a quality regional retail and entertainment area by defining the roadway network, location of land uses, servicing needs and development phasing; and
- Ensure efficient and phased implementation of the Plan.

### 1.4 VISION

The vision and guiding principles for the ASP area outline the aspirations of Regional Municipality of Wood Buffalo, its residents, and stakeholders. They have been developed through stakeholder and public engagement, as well as review of relevant policies and site information. The policies aim to inform development to achieve the guiding principles, and ultimately the vision of the ASP. The vision for the ASP is:

The Athabasca Power Centre Area Structure Plan will guide the development of a regional shopping area that provides a mix of retail, entertainment and other complementary uses at varying scales, while respecting the natural environment. The development will allow efficient vehicular circulation, while enhancing pedestrian connections through a site that is comprehensively designed to include a vibrant and high quality public realm.

*"We want new big box stores and variety."*

*"We want a place to spend our time, not just our money: a place for families to stay and enjoy, for more than shopping."*

*"Respect the community you're building: nature and people."*

*"We want new retail stores and restaurants."*

*"We want this to be an attractive place."*

*"We want more than stores and parking."*

Comments provided by members of the public during an open house held on March 28, 2014.

## PART 2: ACHIEVING THE VISION

### PRINCIPLE 1: CREATE A DIVERSE, REGIONAL COMMERCIAL DEVELOPMENT WITH A FOCUS ON LARGE FORMAT RETAIL

#### OBJECTIVE 1: PROVIDE FOR A MIX OF COMMERCIAL USES

- 2.1.1 Development shall consist of a mix of retail development, large format retail, and supporting land uses.
- 2.1.2 Clustering and/or combining of retail uses into a single structure(s) should be considered to enhance walkability and maximize space.
- 2.1.3 Any potential additional housing requirement generated by this development shall be provided offsite.

### PRINCIPLE 2: RESPECT SENSITIVE ENVIRONMENTAL FEATURES

#### OBJECTIVE 1: RESPECT THE NATURAL ENVIRONMENT

- 2.2.1 The riparian and habitat characteristics of Goat's Head Creek shall be respected.
- 2.2.2 The space created between buildings/structures and the Athabasca River shall be safe, well lit and attractive.
- 2.2.3 A minimum 100 metre setback from the top-of-bank of the Athabasca River shall be required.
- 2.2.4 For new development or subdivision applications adjacent to environmentally sensitive features (including Goat's Head Creek), a minimum riparian setback shall be determined by a geotechnical and biophysical assessment performed by a qualified professional.

- 2.2.5 An environmental assessment may be required to ensure all measures are taken to mitigate any potential environmental impacts.
- 2.2.6 Only activities that have limited impact on streams and rivers will be allowed within the riparian setback. These activities include but are not limited to walking trails, and structures and equipment associated with day use areas.
- 2.2.7 Applications adjacent to the riparian setback will only be considered after demonstrating that development or subdivision will not adversely affect the water quality.

#### OBJECTIVE 2: MITIGATE FLOOD + ICE JAM HAZARDS

- 2.2.8 Development shall be designed to mitigate the impact of flooding and ice jams.

### PRINCIPLE 3: CREATE A SENSE OF PLACE THROUGH QUALITY PUBLIC SPACES AND URBAN DESIGN

#### OBJECTIVE 1: ACHIEVE A HIGH QUALITY OF URBAN DESIGN

- 2.3.1 "Winter City" principles should be incorporated, where appropriate.
- 2.3.2 Building finishes shall be attractive, complementary, high quality, and responsive to the challenges of a northern climate.
- 2.3.3 Building façades should include windows, and vertical and horizontal articulation to improve aesthetics.
- 2.3.4 Building entrances shall be visually prominent and include weather protection.
- 2.3.5 Outdoor lighting shall be designed to provide a safe environment and support a pleasant shopping experience.
- 2.3.6 The site shall be designed to promote accessibility through universal design.
- 2.3.7 Site design and development should create or allow for views and access to the Athabasca River, Goat's Head Creek, and any other environmental amenities.
- 2.3.8 Public art should be encouraged throughout the site and could reflect local culture, heritage and/or wildlife themes.



**FIGURE 5: ARTICULATION OF BUILDING FAÇADES, SKETCH BY DANIEL LEAL**

The above conceptual illustration demonstrates visual articulation of building façades using windows, structural elements, façade recesses and different materials. Safe pedestrian access throughout the site is achieved through raised crosswalks. Transparent storefronts contribute to the vitality of the streets.



**FIGURE 6: EXAMPLE OF AN OUTDOOR AMENITY SPACE, PIONEER COURTHOUSE SQUARE IN PORTLAND, OREGON, PHOTO BY BEN MEENG**

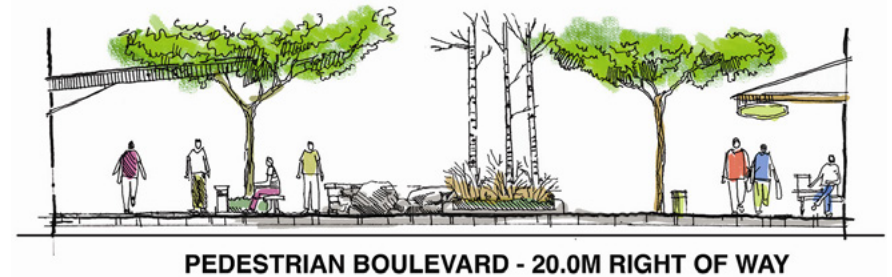


## OBJECTIVE 2: CREATE AMENITY SPACES FOR GATHERING

- 2.3.9 Amenity space(s) shall include a pedestrian boulevard.
- 2.3.10 An indoor amenity space(s) should be considered. This could include a plaza, atrium or other indoor space.
- 2.3.11 Amenity spaces will be encouraged to be designed to host community events and celebrations.
- 2.3.12 Amenity spaces shall be designed to be comfortable year-round.
- 2.3.13 Site furnishings should be provided in amenity spaces to enhance the pedestrian experience.
- 2.3.14 Public art should be provided in amenity spaces.
- 2.3.15 Crime Prevention Through Environmental Design (CPTED) principles should be incorporated to enhance safety.

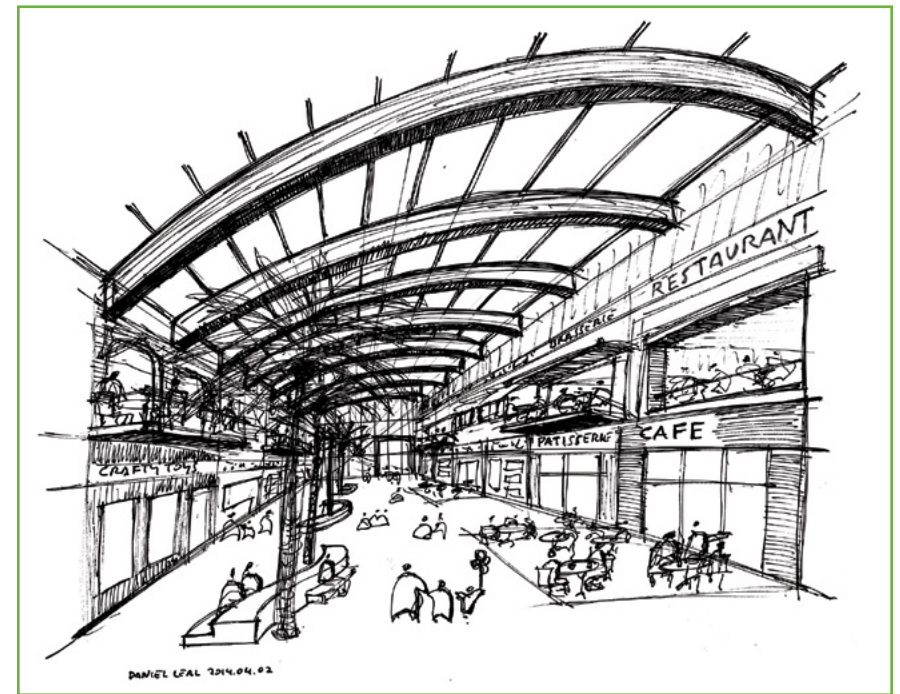
## OBJECTIVE 3: PROVIDE ATTRACTIVE LANDSCAPING + BUFFERING

- 2.3.16 Attractive landscaping shall be provided at all site entrances and along all roadways.
- 2.3.17 Landscaping shall be used to break up parking lots and to enhance public amenity spaces.
- 2.3.18 Drought-tolerant and native plant material shall be prioritized, where possible.
- 2.3.19 Buffering between the TaigaNova Eco-Industrial Park and the site shall be provided.
- 2.3.20 Maintenance of any buffering materials or structures should be the responsibility of the owner and a condition of the development permit.



**FIGURE 7: CONCEPTUAL CROSS SECTION OF THE PEDESTRIAN BOULEVARD, SKETCH BY MIKO BETANZO**

This is a conceptual illustration of a pedestrian boulevard. Several design and amenity features are demonstrated, including covered walkways, landscaping and seating.



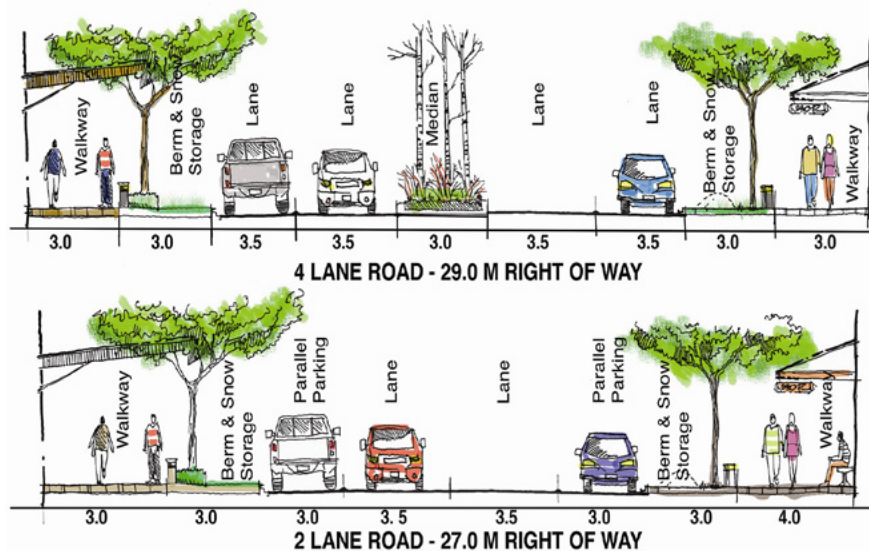
**FIGURE 8: INDOOR AMENITY SPACE (WINTER HALL), SKETCH BY DANIEL LEAL**

This is a conceptual illustration showing a possible indoor amenity space with commercial areas and indoor patios spilling out onto a central atrium.

## PRINCIPLE 4: PROVIDE SAFE, COMFORTABLE + EFFICIENT MOVEMENT THROUGHOUT THE SITE

### OBJECTIVE 1: PROVIDE CONVENIENT VEHICULAR ACCESS + CONNECTIVITY

- 2.4.1 Provide safe and efficient vehicular movement between shops, amenity spaces and parking areas.
- 2.4.2 Direct access to the site shall not be provided from Highway 63.
- 2.4.3 A minimum of two access/egress points shall be provided to the site.
- 2.4.4 The internal road network shall be designed and submitted as part of an Outline Plan application. Where appropriate, innovative approaches to internal road network design may be considered.



**FIGURE 9: CONCEPTUAL CROSS SECTIONS OF INTERNAL ROADS, SKETCH BY DANIEL LEAL**

The above cross sections conceptually illustrate street design features, including landscaping, sheltered walkways and snow storage areas.

### OBJECTIVE 2: PROVIDE SAFE PEDESTRIAN + CYCLIST CIRCULATION

- 2.4.5 Pedestrian access within the site shall be prioritized.
- 2.4.6 Convenient, well-located and marked pedestrian crossings shall be provided to improve safety and deter crossings in unmarked areas.
- 2.4.7 Sidewalks should be provided on both sides of roadways.
- 2.4.8 Covered and/or sheltered walkways will be encouraged, where possible.
- 2.4.9 Covered and/or sheltered walkways should be provided between buildings.
- 2.4.10 Pedestrian and cycling connections to existing or future Municipal trail networks shall be provided.
- 2.4.11 A minimum of one pedestrian pathway connecting the site with TaigaNova Eco-Industrial Park should be provided.
- 2.4.12 Infrastructure necessary to support public transit shall be provided once public transit service to the site is established.
- 2.4.13 Bicycle parking shall be provided in convenient locations.
- 2.4.14 All drive throughs will be required to be located away from amenity spaces and access/egress points into and out of the Power Centre site.

### OBJECTIVE 3: PROVIDE ACCESS BY PUBLIC TRANSIT

- 2.4.15 Provide the necessary infrastructure to support public transit.
- 2.4.16 At least one centrally located transit stop that minimizes walking distances shall be required.



2.4.17 Transit stops shall have adequate weather protection.

**OBJECTIVE 4: SCREEN LOADING + STORAGE AREAS**

2.4.18 Waste management areas and recycling areas shall be visually screened and designed to deter animal access.

2.4.19 Loading areas should be located to the rear of buildings. If this cannot be achieved, then they shall be screened.

2.4.20 Outdoor storage areas shall be screened.

**OBJECTIVE 5: PROVIDE WELL-DESIGNED + SAFE PARKING**

2.4.21 Shaded surface parking will be encouraged, where possible.

2.4.22 Surface parking shall use landscaping to create visual articulation.

2.4.23 Separated walkways that are oriented parallel to drive aisles are encouraged in order to enhance pedestrian safety.

## PRINCIPLE 5: PROVIDE EFFICIENT SERVICING + INFRASTRUCTURE THAT SERVES THE FULL BUILD-OUT OF THE SITE

### OBJECTIVE 1: ENSURE PROPER COORDINATION OF GENERAL MUNICIPAL SERVICES + UTILITIES

- 2.5.1 As part of an Outline Plan, an Engineering Design Brief shall be required, outlining geotechnical and biophysical information, a Water Network Analysis, Sanitary Sewer Modelling, Master Drainage Plan and Stormwater Management Plan.
- 2.5.2 The site shall be serviced with municipal water, sanitary sewer, a stormwater system, and shallow utilities (i.e., gas, cable, electricity, telephone).
- 2.5.3 Provision, alignment and capacity of water, sanitary sewer, and stormwater services shall be in accordance with Engineering Servicing Standards, based upon utility servicing studies and analysis.
- 2.5.4 The location of all utilities and the provision of right-of-ways, easements and related line alignments shall be in accordance with Engineering Servicing Standards.
- 2.5.5 At time of subdivision or development permit, an applicant shall enter into an agreement to provide utility right-of-ways or easements in accordance with Engineering Servicing Standards.
- 2.5.6 Utility right-of-ways, easements, and public utility lots shall be provided as required to accommodate the development or the extension of municipal utilities necessary for development.
- 2.5.7 Utility rights-of-way and easements should be located to ensure the long-term viability of street trees.

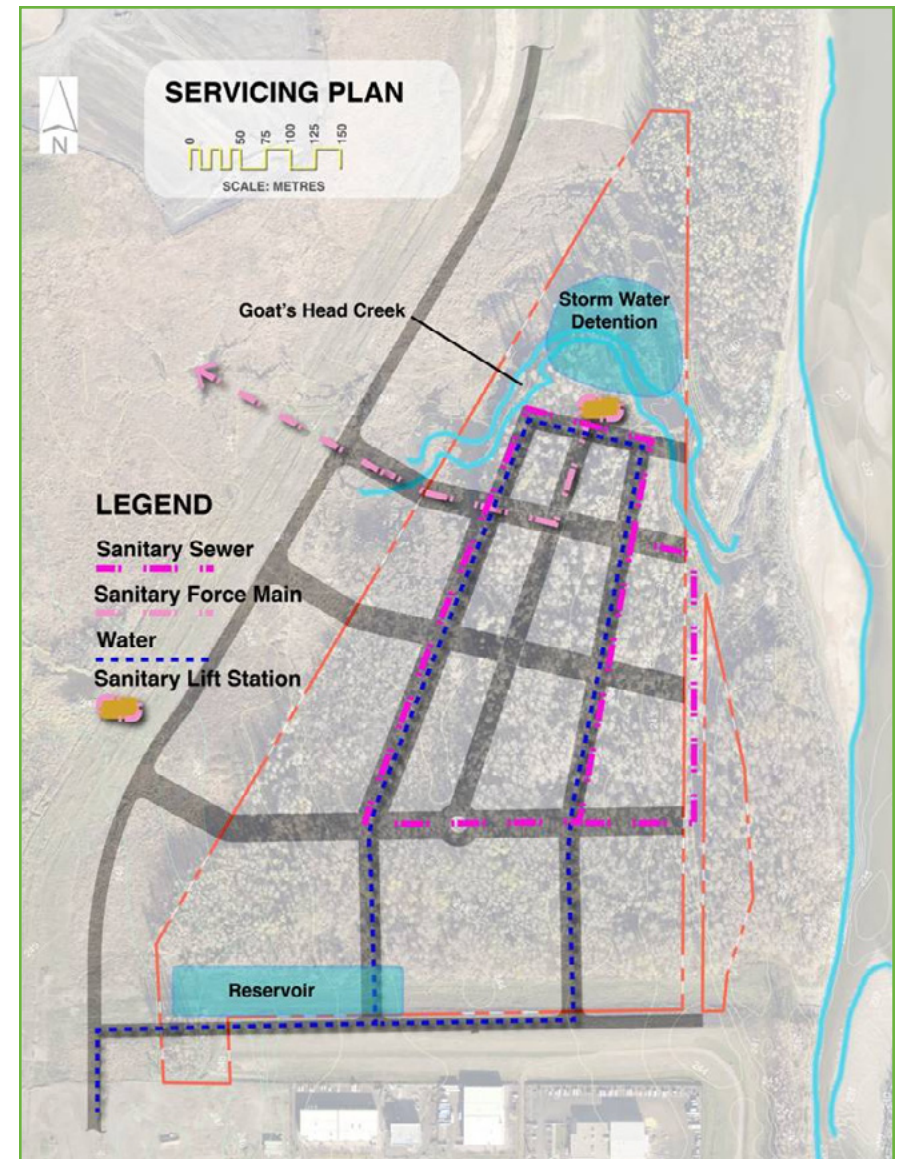


FIGURE 10: CONCEPTUAL SERVICING PLAN

Note: For illustrative purposes. Exact plan to be determined at the Outline Plan stage.

2.5.8 As part of an Outline Plan approval, the applicant shall submit studies and information determined necessary to identify the location and alignment requirements for services and utilities within the development.

#### OBJECTIVE 2: PROVIDE WATER SERVICING

2.5.9 An onsite reservoir and pumphouse shall be identified by the applicant as part of an Outline Plan application.

2.5.10 The water distribution system shall be designed to adequately, safely and efficiently serve the full build-out of the site.

#### OBJECTIVE 3: PROVIDE SANITARY SEWER SERVICING

2.5.11 An onsite lift station for wastewater shall be identified as part of an Outline Plan application.

2.5.12 The sanitary sewer system shall be designed to adequately, safely and efficiently serve the full build-out of the site.

#### OBJECTIVE 4: PROVIDE STORMWATER MANAGEMENT

2.5.13 The stormwater management system shall be designed to adequately and efficiently serve the full build-out of the site.

2.5.14 Stormwater ponds shall respect environmentally sensitive areas.

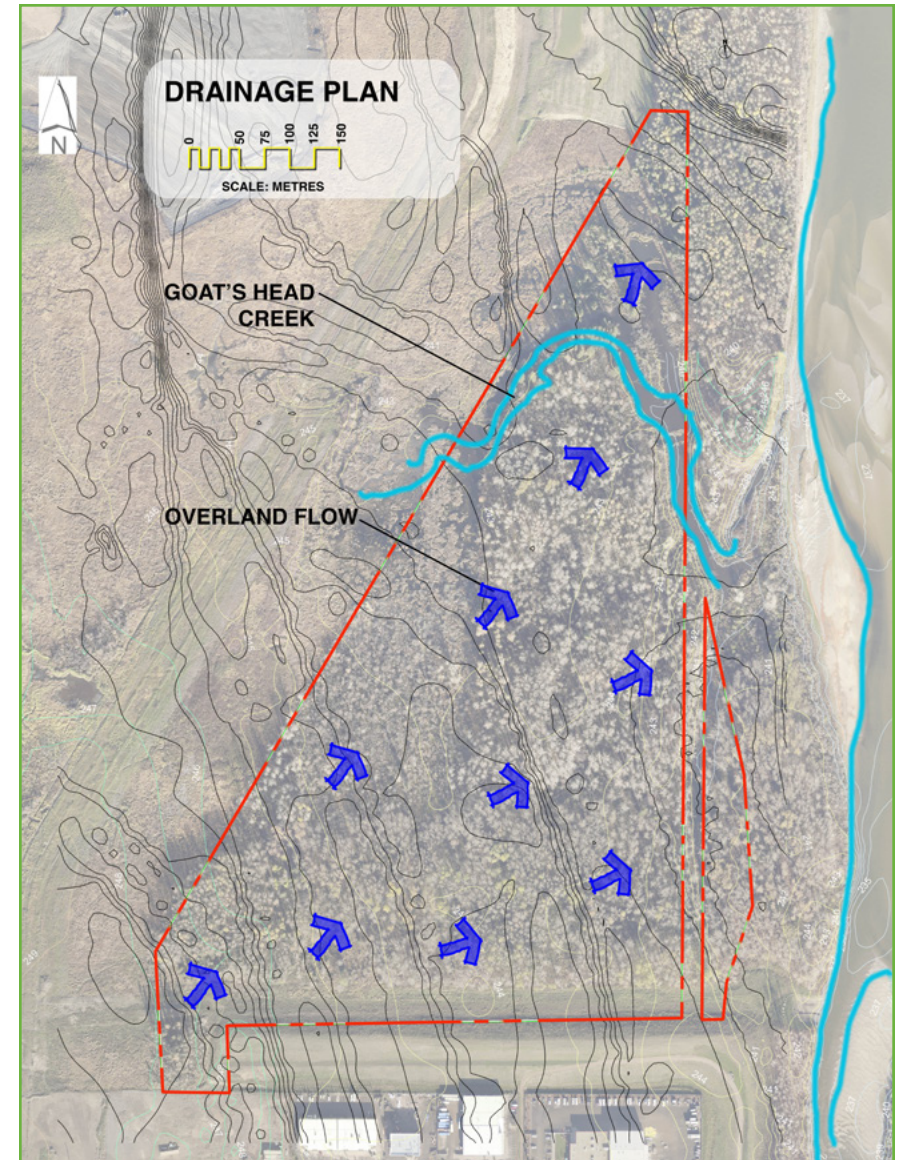


FIGURE 11: EXISTING DRAINAGE PATTERN FOR THE ATHABASCA POWER CENTRE



- 2.5.15 Mitigation measures to improve water quality should be introduced if stormwater enters existing watercourses.
- 2.5.16 Low Impact Development methods should be considered to mitigate the effects of stormwater runoff into local watercourses.

#### **OBJECTIVE 5: PROMOTE SUSTAINABLE BUILDINGS + INFRASTRUCTURE**

- 2.5.17 Alternative energy and heating infrastructure should be considered during site and building design.
- 2.5.18 Energy and water conservation and the reduction of greenhouse gas emissions should be encouraged through building design.
- 2.5.19 At the time of an Outline Plan application, an applicant should contact the Municipality to discuss opportunities for reclaimed water use.

#### **OBJECTIVE 6: PROVIDE FOR ADEQUATE SNOW STORAGE**

- 2.5.20 An onsite snow storage plan shall be provided and shall demonstrate how parking, pedestrian walkways, landscaping, drainage, and emergency access will not be impeded. If these objectives cannot be achieved, removal of snow offsite shall be required.

## PART 3: IMPLEMENTATION

### 3.0 DEVELOPMENT PHASING

- 3.0.1 If development phasing is to occur, it shall be determined at the Outline Plan stage.

### 3.1 IMPLEMENTATION

- 3.1.1 The provision and programming of amenity space may occur through partnerships with private parties as well as government.
- 3.1.2 At the time of land use re-designation (rezoning), subdivision or development permit application, additional technical information may be required in order to confirm the development feasibility.
- 3.1.3 Onsite costs associated with new roadways and infrastructure shall be borne by the developers, unless as per development agreements, other cost sharing approaches are identified.
- 3.1.4 Partnerships and collaborative processes should be encouraged between the developer, the Municipality and other parties in the provision of offsite infrastructure and onsite amenity spaces and/or environmental features.
- 3.1.5 Municipal funding may be considered for a portion of the service road required for site access and egress if the developer commits to the provision of amenity spaces and/or environmental features.
- 3.1.6 Development incentives should be considered for developers who implement sustainable design practices.
- 3.1.7 Closure of the public road allowance may be required.

### 3.2 OUTLINE PLAN

- 3.2.1 An Outline Plan for the entire site shall be required.
- 3.2.2 The Outline Plan shall include the following:
- i. A statement of compliance with the Municipal Development Plan, this Area Structure Plan, and the Land Use Bylaw;
  - ii. A phasing plan;
  - iii. A biophysical assessment of physical and environmental characteristics, including vegetation, fish habitat, watercourses, and topographic contours (at 1 metre contours). This assessment shall also make recommendations for development setbacks from Goat's Head Creek and shall be undertaken by a qualified professional licensed to practice in Alberta;
  - iv. A detailed geotechnical study to confirm geology, the location of the top-of-bank and required setbacks as well as addressing any other geotechnical limitations and which could inform types of development allowable on the site. This assessment shall be undertaken by a qualified professional licensed to practice in Alberta;
  - v. An environmental site assessment that identifies environmentally sensitive features and contaminated sites. This assessment shall be undertaken by a qualified professional licensed to practice in Alberta;
  - vi. Road network and public transit routes;
  - vii. Arterial, collector and local road alignments and cross sections;

- viii. A Transportation Impact Assessment (TIA) undertaken by a qualified professional licensed to practice in Alberta;
- ix. An Engineering Design Brief that is in accordance with Engineering Servicing Standards and includes a Water Network Analysis, Sanitary Sewer Modelling, Master Drainage Plan and Stormwater Management Plan;
- x. Sanitary Sewer Servicing Study;
- xi. Snow storage plan; and
- xii. Any other studies the Municipality deems necessary.

### **3.3 AMENDMENTS**

- 3.3.1 The Municipality shall recommend and work with the Province to amend the Urban Service Area boundary to include the Athabasca Power Centre site.
- 3.3.2 A Land Use Bylaw (Bylaw No. 99/059) Amendment shall be prepared to help implement this ASP.
- 3.3.3 The Highway 63 North Area Structure Plan (Bylaw No. 99/037) shall be amended to exclude the Athabasca Power Centre site.
- 3.3.4 External applicants applying to amend the ASP shall provide a supporting technical report so that the proposed changes can be

properly evaluated. The technical report shall outline the following:

- i. Justification for the amendment;
- ii. The extent to which existing areas for the proposed use are available for development;
- iii. The cumulative effects that the proposed amendment and related development may have on the natural environment and surrounding land uses;
- iv. The effect the proposed amendment may have on road, water, sewer, and stormwater systems; and
- v. Any other consideration the Municipality deems necessary.

- 3.3.5 The Municipality shall undertake to review and update, if necessary, the ASP at regular intervals from the date of adoption. This review should determine whether any changes are required to the current land use designations.

# GLOSSARY OF TERMS

## **Access/Egress Points**

A driveway or other opening allowing vehicles to enter and exit to and from the street.

## **Alternative Energy and Heating Infrastructure**

Alternative heating and energy infrastructure employs technologies utilizing renewable resources in order to provide sustainable service provisioning

## **Amenity Space**

An indoor or outdoor area provided for active or passive recreation and enjoyment of the occupants of a development, which may be for private or communal use and owned individually or in common.

## **Area Structure Plan (ASP)**

A plan adopted by Municipal Council as a bylaw in accordance with the Municipal Government Act that provides a framework for future development of an area or community. It includes general land uses, proposed access and municipal services.

## **Buffer**

A natural or linear area comprising shrubs, trees, earth berms or physical fencing that provides visual or physical separation and/or noise attenuation between water bodies, lots, roads, utility corridors and other uses.

## **Conceptual Site Plan**

A generalized plan indicating the boundaries of a tract or tracts of land under common ownership, and identifying proposed land use, land use intensity, and thoroughfare alignment.

## **Crime Prevention Through Environmental Design (CPTED)**

A multi-disciplinary approach to deterring criminal behaviour and reducing fear of crime through the design of the built environment.

## **Development Incentives**

Measures taken by a municipality to encourage certain types of development.

## **Development Phasing**

Establishing the logical progression of development to accommodate the principles defined in the Area Structure Plan. The Municipal Government Act requires that an Area Structure Plan describe the sequence of development.

## **Engineering Servicing Standards**

Regional Municipality of Wood Buffalo standards that define the minimum expectation for public infrastructure.

## **Environmental Assessment**

A report that assesses all environmental characteristics of an area and determines what effects or impacts will result if the area is altered or disturbed by a proposed action. In instances where an Environmental Impact Assessment is not required under federal or provincial law, an environmental assessment may be required to help the Municipality consider the environmental impacts of a proposed plan, and consider alternatives or appropriate mitigation strategies during the planning stage.

## **Full Build-Out**

The development of land to its full potential or theoretical capacity as permitted under the Area Structure Plan.

## **Geotechnical Study**

A study that identifies geotechnical risks and mitigation measures, carried out by an Accredited Professional, which must acknowledge that the Municipality may rely upon the study when making decisions.

## **Land Use Bylaw (LUB)**

A bylaw adopted by Municipal Council pursuant to the Municipal Government Act for the establishment of land use districts and the regulation of development, used for implementing statutory plan policies including the Municipal Development Plan, Area Structure Plans and Area Redevelopment Plans.

## **Large Format Commercial**

Large format commercial means a commercial facility used for the retail sale or wholesaling of goods to the general public occupying a building of no less than 5,000 m<sup>2</sup> Gross Floor Area.

## **Low Impact Development Methods**

Innovative stormwater management which aims to mimic a site's predevelopment hydrology. LID includes design techniques that infiltrate, filter, store, evaporate and detain runoff close to its source. By implementing such approaches, water can be managed in a way that reduces the impact of built areas and promotes the natural movement of water within an ecosystem or watershed.

## **Municipal Development Plan (MDP)**

A plan that functions as a municipality's overall policy guide for future growth and development. It is a statutory plan adopted by a Municipal Council under the authority of Section 632 of the Municipal Government Act. The plan outlines the direction and scope of future development, the provision of required transportation systems and municipal services, the coordination of municipal services and programs, environmental matters, and economic development.

## **Outline Plan**

A plan providing a more specific planning framework for an area included within an Area Structure Plan and which conforms to the general principles and concepts established in the ASP. The Outline Plan focuses on smaller areas of land and provides detail as to how the land will be further subdivided, serviced and built upon.

## **Retail Development**

A commercial facility used for the retail sale of goods to the general public occupying a commercial retail unit of less than 5,000 m<sup>2</sup> Gross Floor Area.

## **Riparian Areas**

Ecosystems that border lakes, estuaries and streams that function as transition areas between land and water, and host a wide array of plant and animal life. Riparian preservation plays an important role in the overall health and functional process of a watercourse or water body.

## **Setback**

The minimum required horizontal distance between a development and a property line or any other features of a site, including, but not limited to, lease boundaries, watercourses, slopes, and other environmental features.

## **Sustainable Design Practices**

Designing services and the built environment to comply with the principles of social, economic, and ecological sustainability. The World Commission on the Environment and Development (1987) defines sustainability as "development that meets the needs of the present without compromising the ability of future generations to meet their own needs."

## **Top of Bank**

The point closest to the boundary of the active floodplain of a lake, stream, or other body of water where a break in slope of the land occurs such that the grade beyond the break is flatter than 3 (horizontal) to 1 (vertical) at any point for a minimum of 15 metres measured perpendicularly from the break. Where banks are not well defined (e.g. in the case of lakes, wetlands



or ponds), the top of the bank is equivalent to the high water mark or active floodplain, whichever is greater.

## **Universal Design**

Place design that promotes equal opportunity for all individuals regardless of age, stature, or ability to use the space. These spaces are designed to accommodate a wide variety of abilities, and minimize barriers to access or use.

## **Urban Service Area**

Fort McMurray, located within the Regional Municipality of Wood Buffalo. Fort McMurray is recognized as equivalent to a city by the Government of Alberta for the purposes of program delivery and grant eligibility.

## **Vertical/Horizontal Articulation**

A method of architectural styling that emphasizes both the vertical and horizontal parts of a building separately, but in a harmonic and balanced manner. This is meant to avoid monotonous facades and introduce visual appeal and variety in a building through, but not limited to, the use of varying colour palettes, projections, recesses, trim, eave and roof lines, and other architectural features.

## **Winter City Principles**

Winter city is a concept for communities in northern latitudes that encourages transportation systems, buildings, and recreation projects to be designed and developed for all four seasons, with particular attention to how they perform during winter.



**BYLAW NO. 14/030**

**BEING A BYLAW OF THE REGIONAL MUNICIPALITY OF WOOD BUFFALO TO AMEND LAND USE BYLAW NO. 99/059**

**WHEREAS** Section 191(1) of the *Municipal Government Act*, R.S.A., 2000, c.M-26 and amendments thereto authorizes Council to adopt a bylaw to amend a Land Use Bylaw.

**NOW THEREFORE**, the Council of the Regional Municipality of Wood Buffalo, in the Province of Alberta, in open meeting hereby enacts as follows:

1. Land Use Bylaw No. 99/059 is hereby amended by redesignating a portion of NE 6-90-09 W4M, NW 5-90-09 W4M, SE 7-90-09 W4M from Parks and Recreation District (PR) to Shopping Centre Commercial District (C3), as depicted in Schedule A.
2. The Chief Administrative Officer is authorized to consolidate this bylaw.
3. This bylaw shall be passed and become effective when it receives third reading and is signed by the Mayor and Chief Legislative Officer.

READ a first time this 9<sup>th</sup> day of July, A.D 2014.

READ a second time this \_\_\_\_\_ day of \_\_\_\_\_, A.D. 2014.

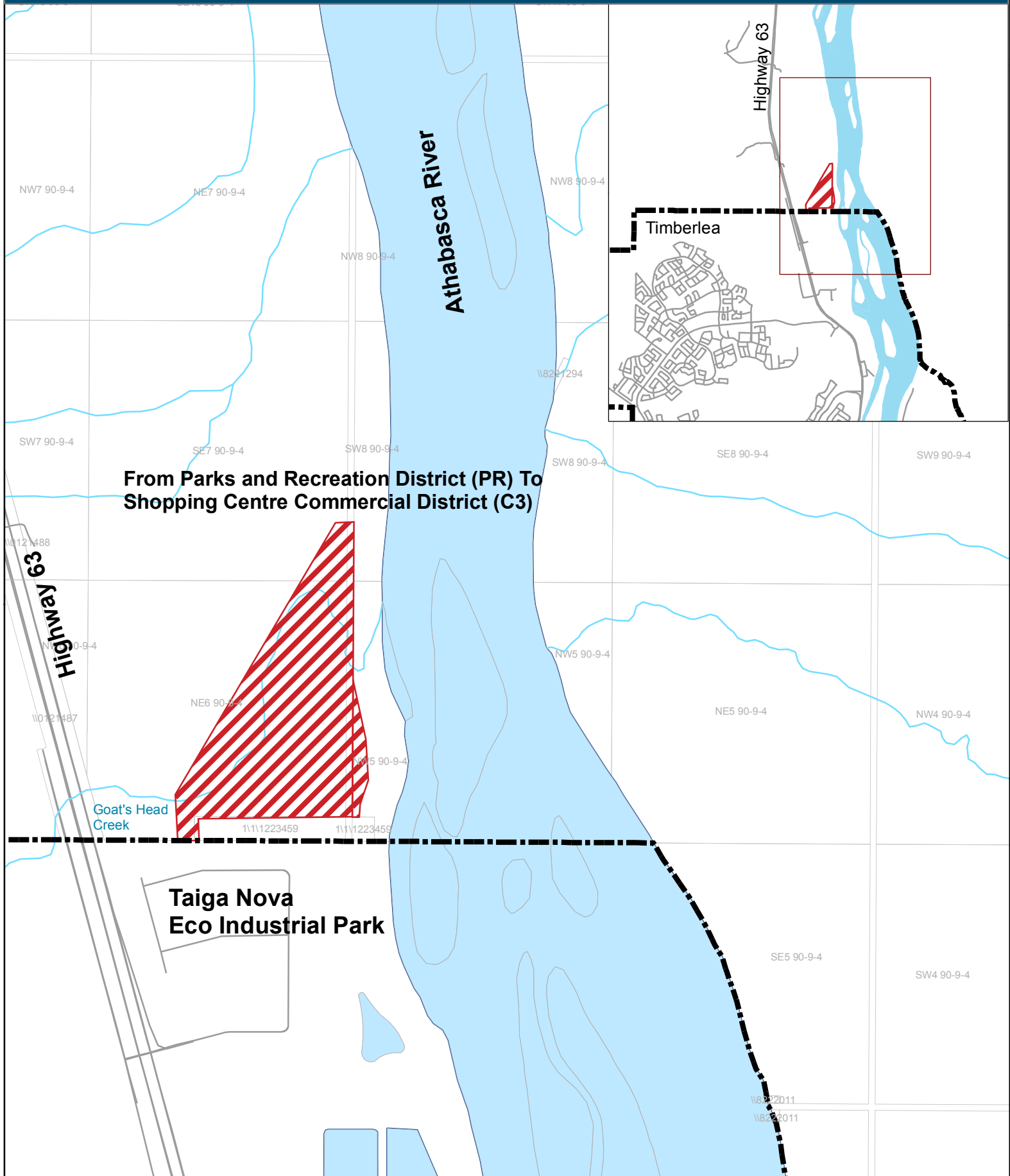
READ a third and final time this \_\_\_\_\_ day of \_\_\_\_\_, A.D. 2014.

SIGNED and PASSED this \_\_\_\_\_ day of \_\_\_\_\_, A.D. 2014.

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Chief Legislative Officer





## Legend



Subject Lands



Urban Service Area

0 210 420 840 Meters

1:15,000



REGIONAL MUNICIPALITY  
OF WOOD BUFFALO





# ATHABASCA POWER CENTRE

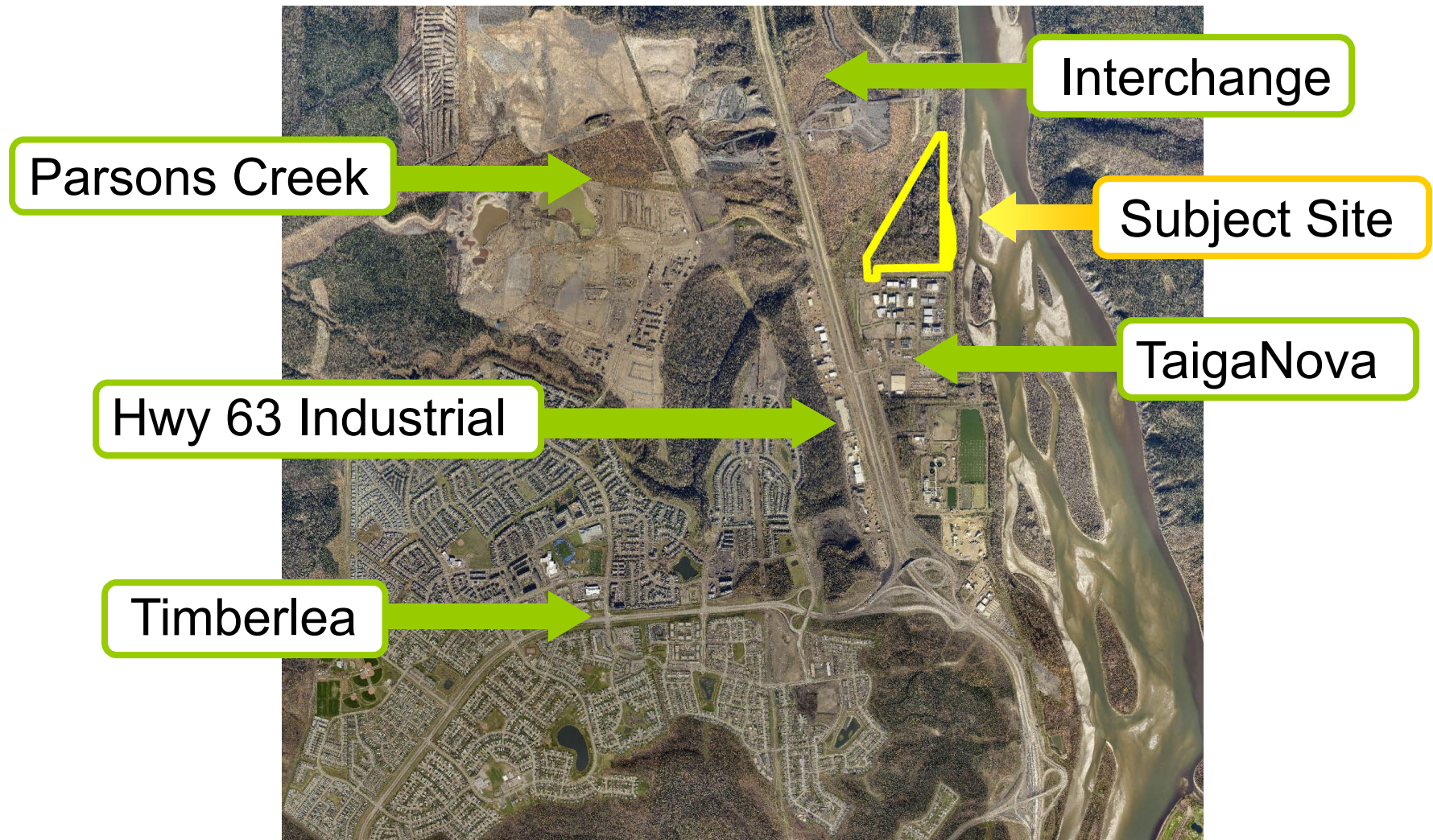
## AREA STRUCTURE PLAN (ASP)

August 26, 2014  
Planning & Development

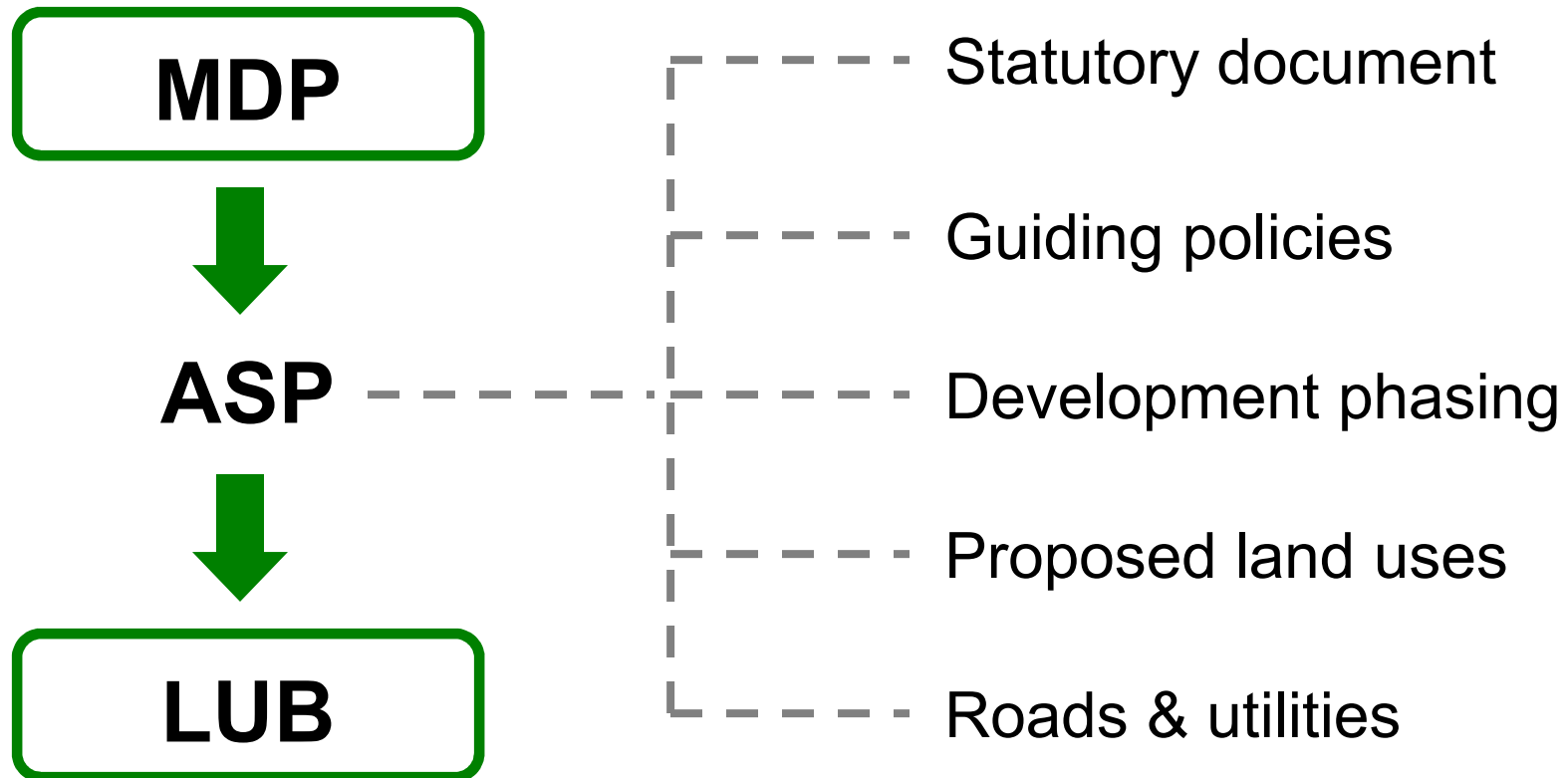
# Purpose

- 1 Adopt Athabasca Power Centre ASP
- 2 Amend Land Use Bylaw
- 3 Repeal Highway 63 North ASP

# Site & Context



# Role of an Area Structure Plan (ASP)



# Rationale

- 1 Commercial & Industrial Land Use Study (CILUS)
- 2 Business Inventory & Gap Analysis
- 3 Municipal Development Plan (MDP)

# Commercial & Industrial Land Use Study (2010)

Actually providing



2.44 m<sup>2</sup> per person

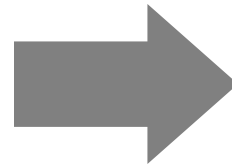
But we need



4.18 m<sup>2</sup> per person



188,500 m<sup>2</sup>  
shortfall



Nearly 1 South  
Edmonton Common

# **Business Inventory & Gap Analysis (2010)**

Food & entertainment

General retail

Sporting goods

Hobby, music, books

Home furnishings

Electronics & appliances

Building & garden supplies



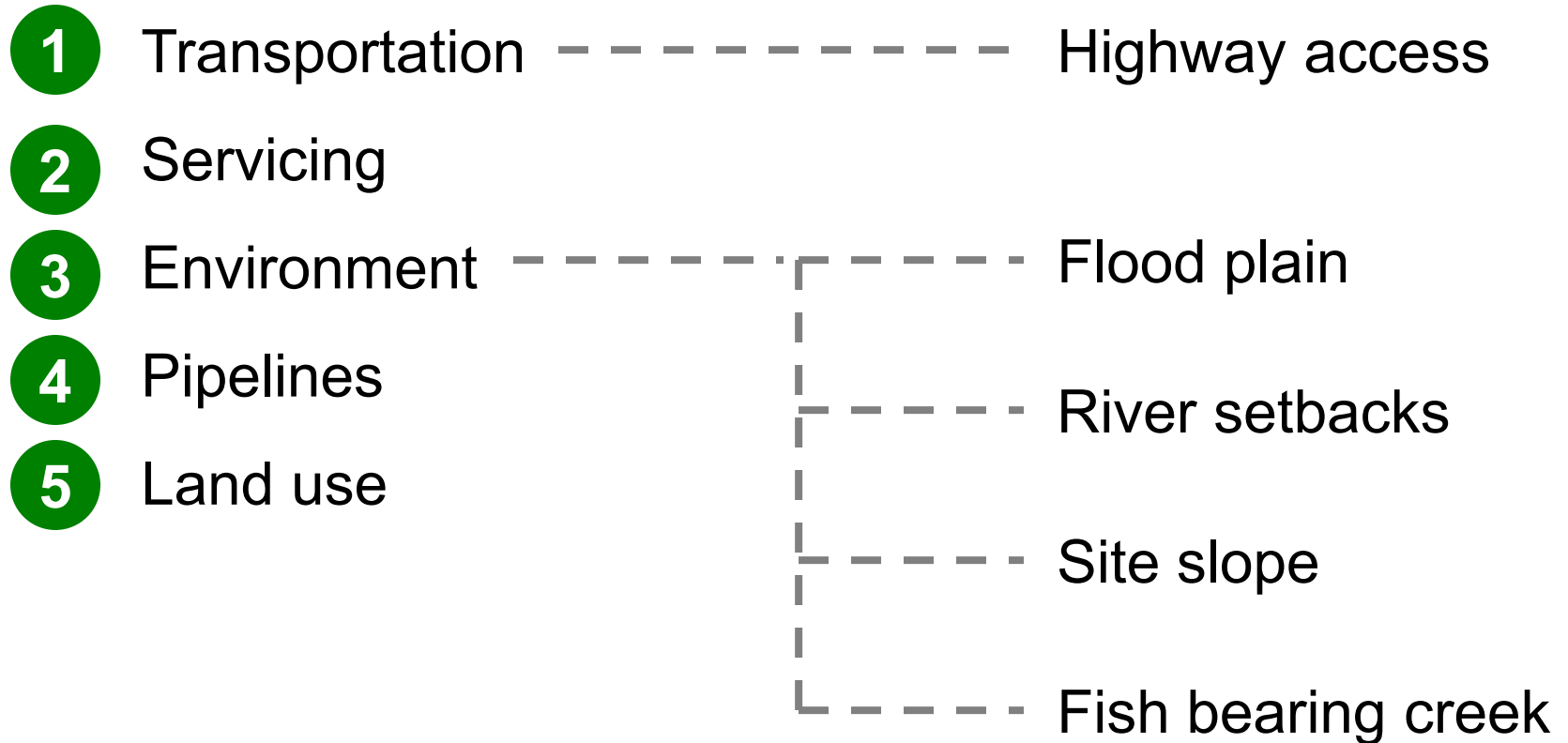
# Municipal Development Plan (2011)



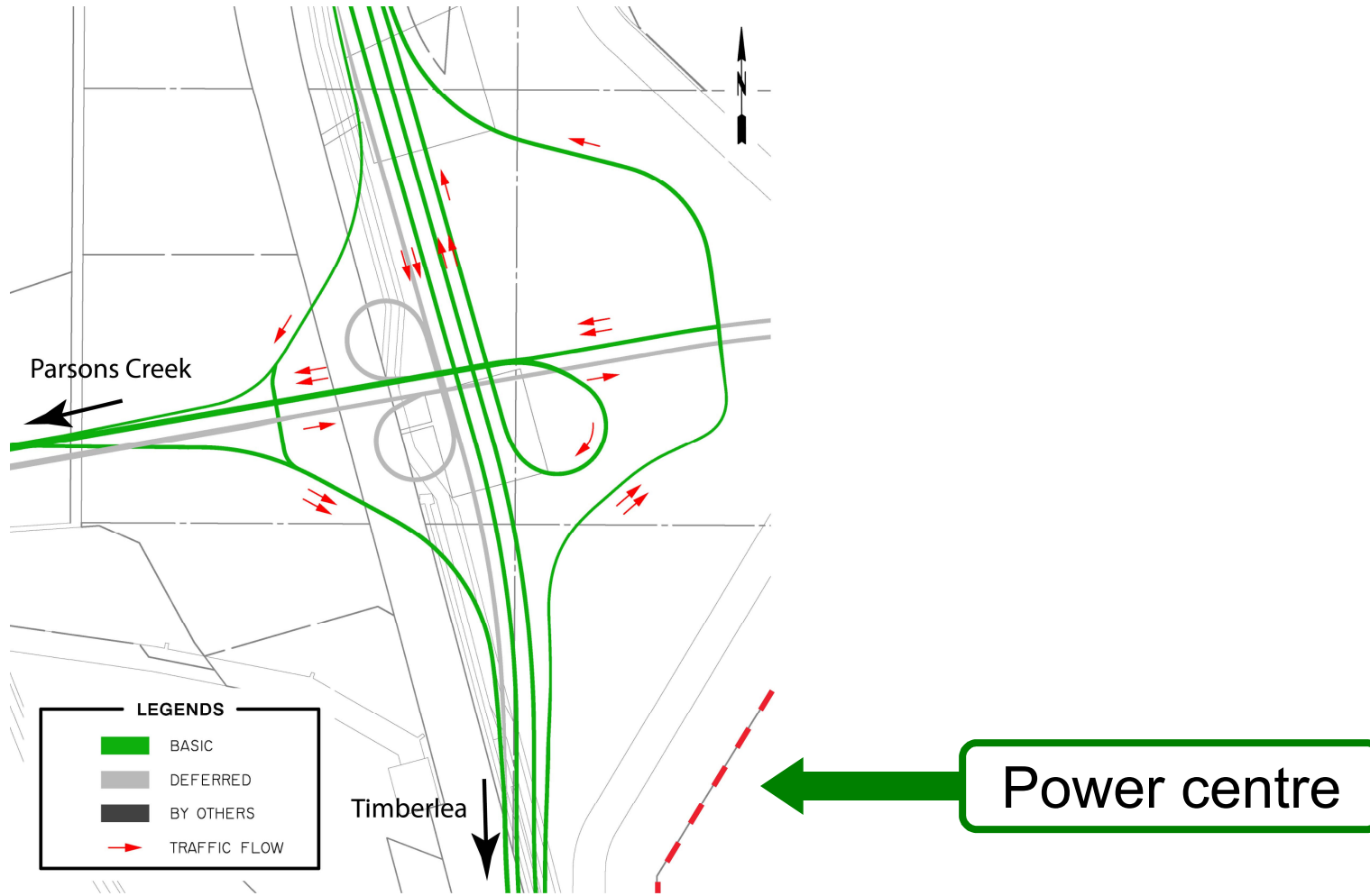
# Creating the ASP

- 1 Background research
- 2 Design charrette
- 3 Public review

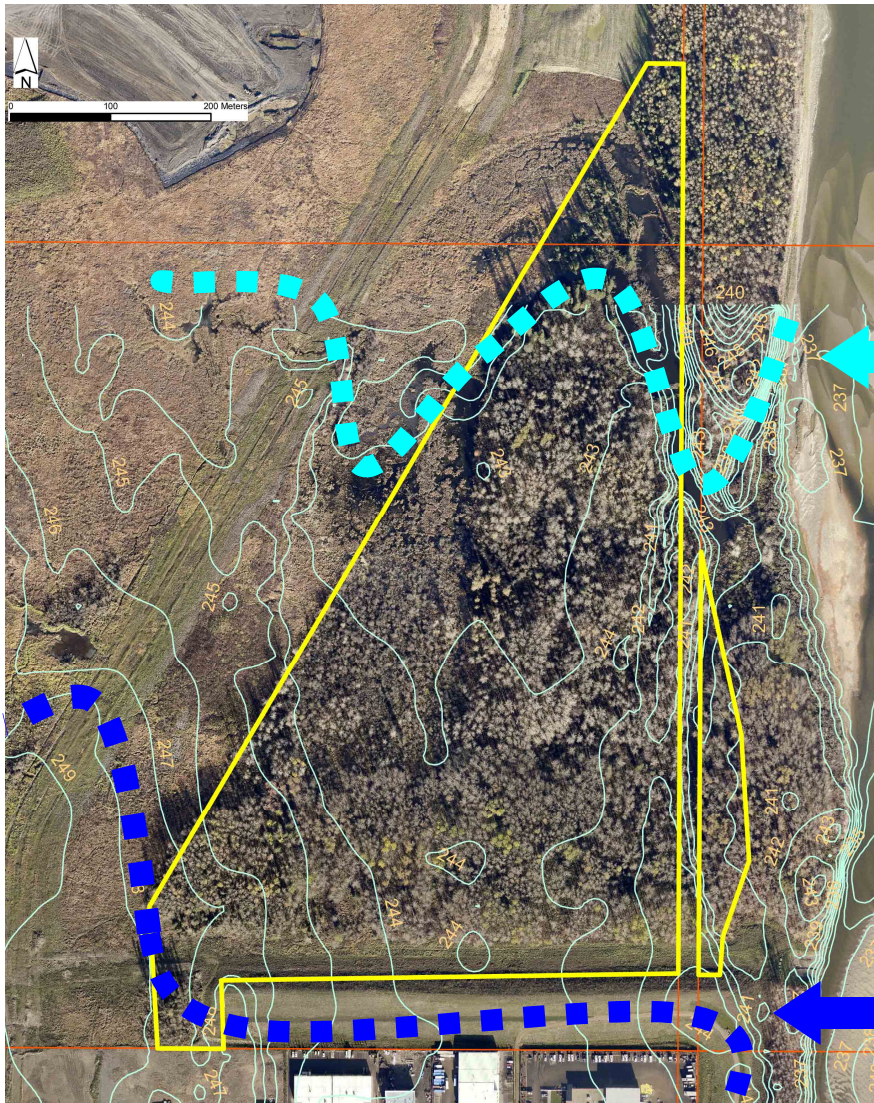
# Background Research



# Transportation: Phase 1 – Parsons Interchange



# Environment



Approximate  
location of  
Goat's Head  
Creek

Approximate  
248 m contour



# Design Charrette



Collaboration



Transparency



Efficiency

## What we did:

Idea generation

Analyzing constraints

Multiple concepts



# Charrette Participants

## Municipal

Community Services  
Culture  
Economic Development  
Emergency Management  
Engineering  
Parks  
Public Operations  
Planning and Development

## External

Alberta Infrastructure  
Alberta Transportation  
HDR Corporation  
CitySpaces Consulting



# Public Participation



# Public feedback

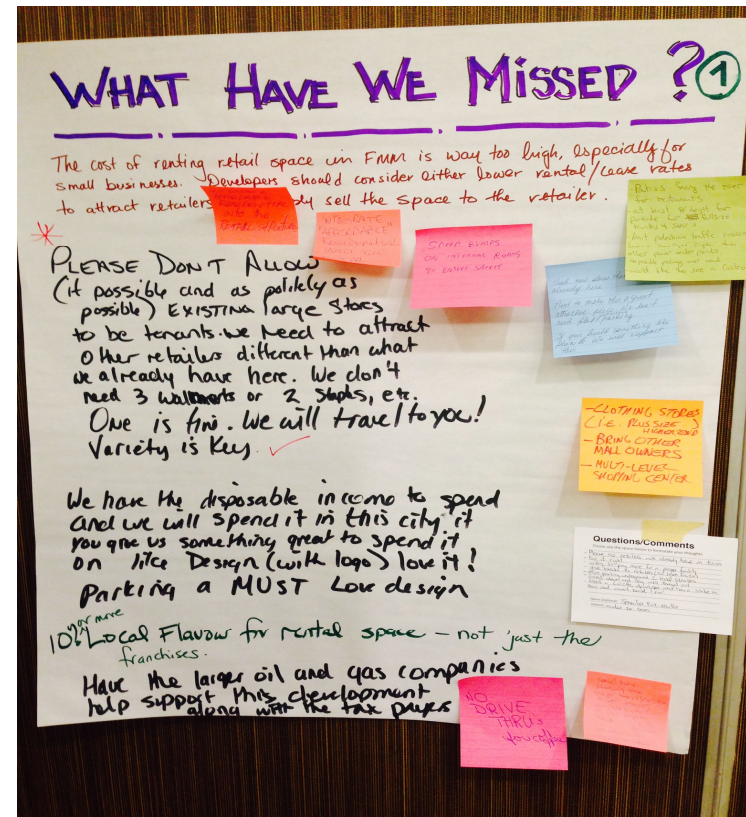


# Developer feedback

## What was critiqued:

## Assumptions & ideas

# Development concepts

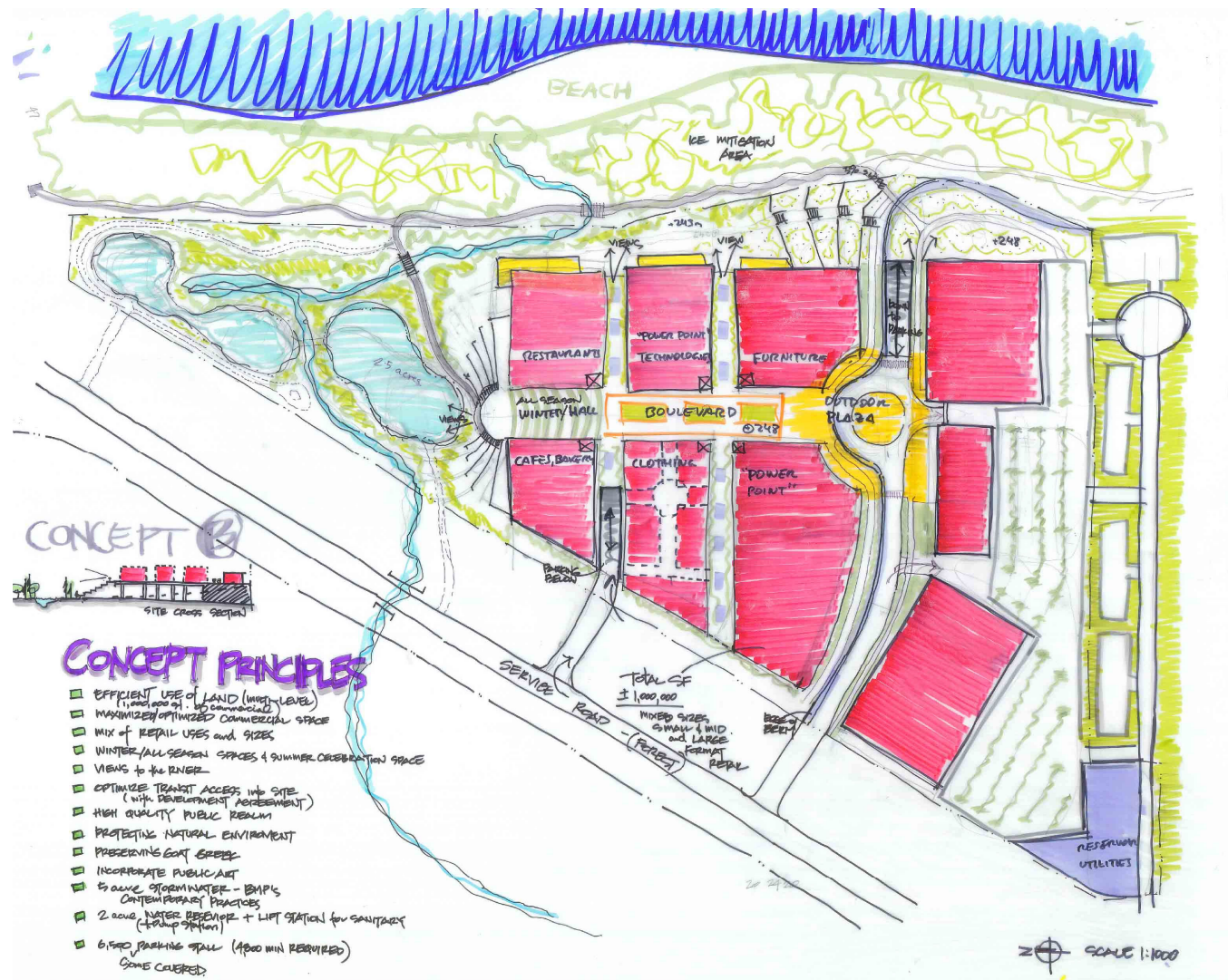


# Charrette Concept #1





# Charrette Concept #2

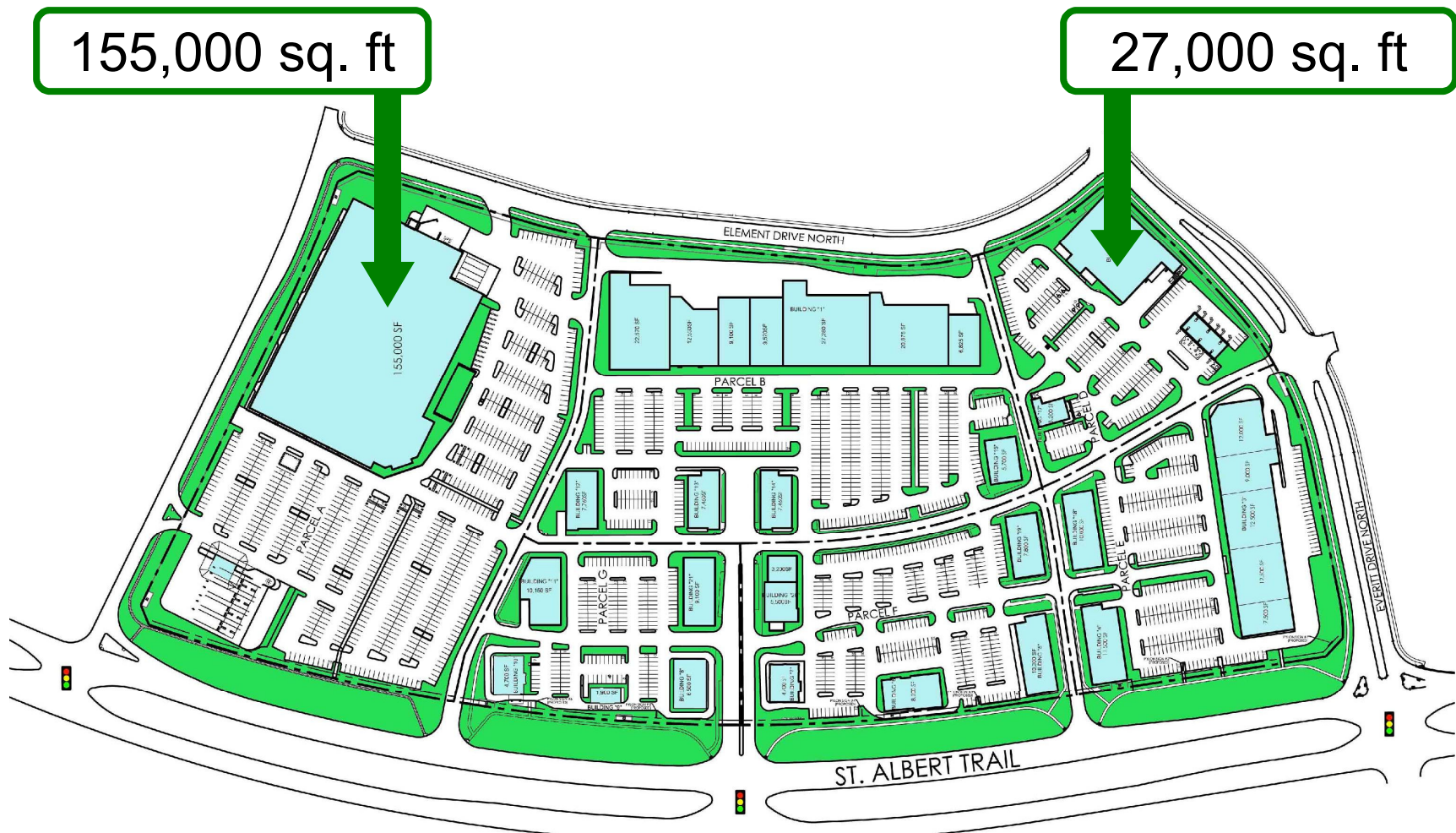


# Hybrid Concept (Approximately 50 net acres)





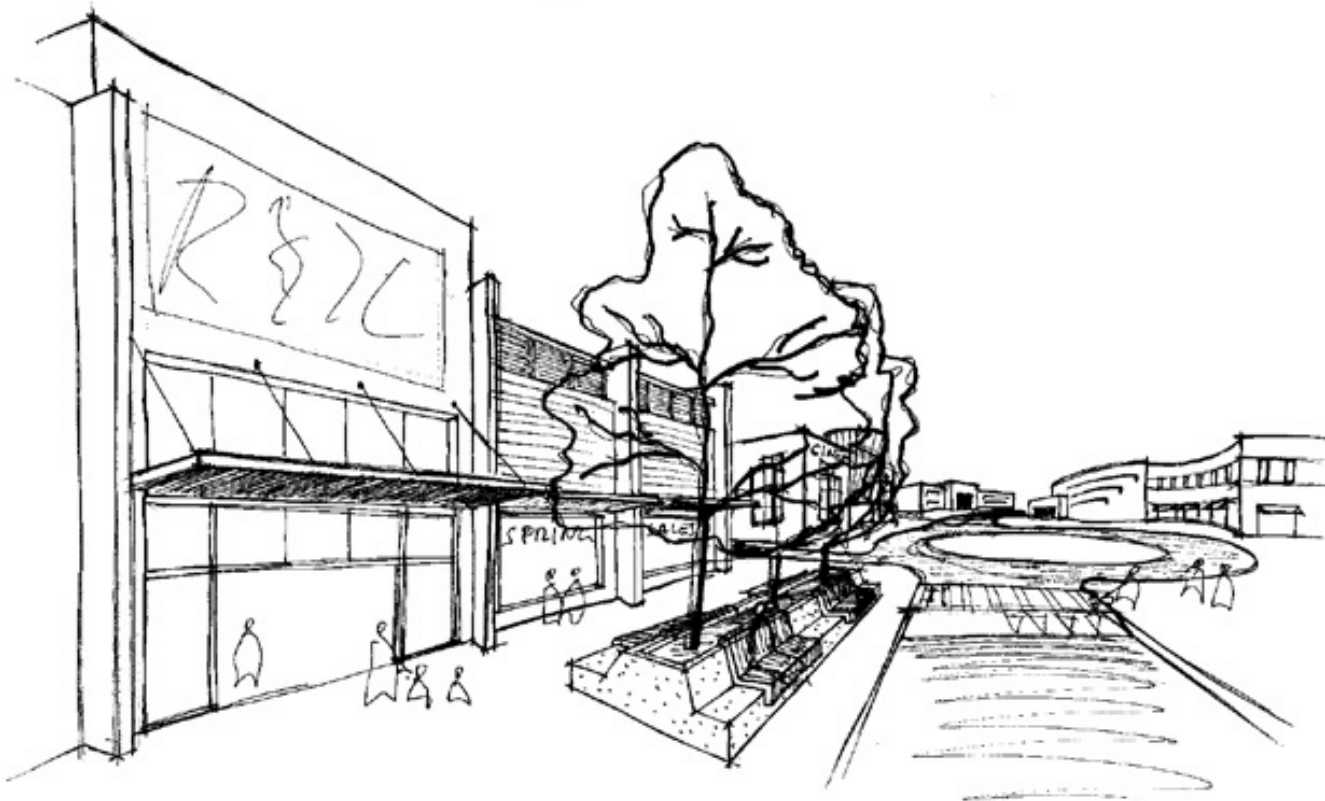
# Example: 50 acre power centre



# Principle 1

Create a diverse, regional commercial development with a focus on **large format retail**

BIG-BOX VOLUME ARTICULATION

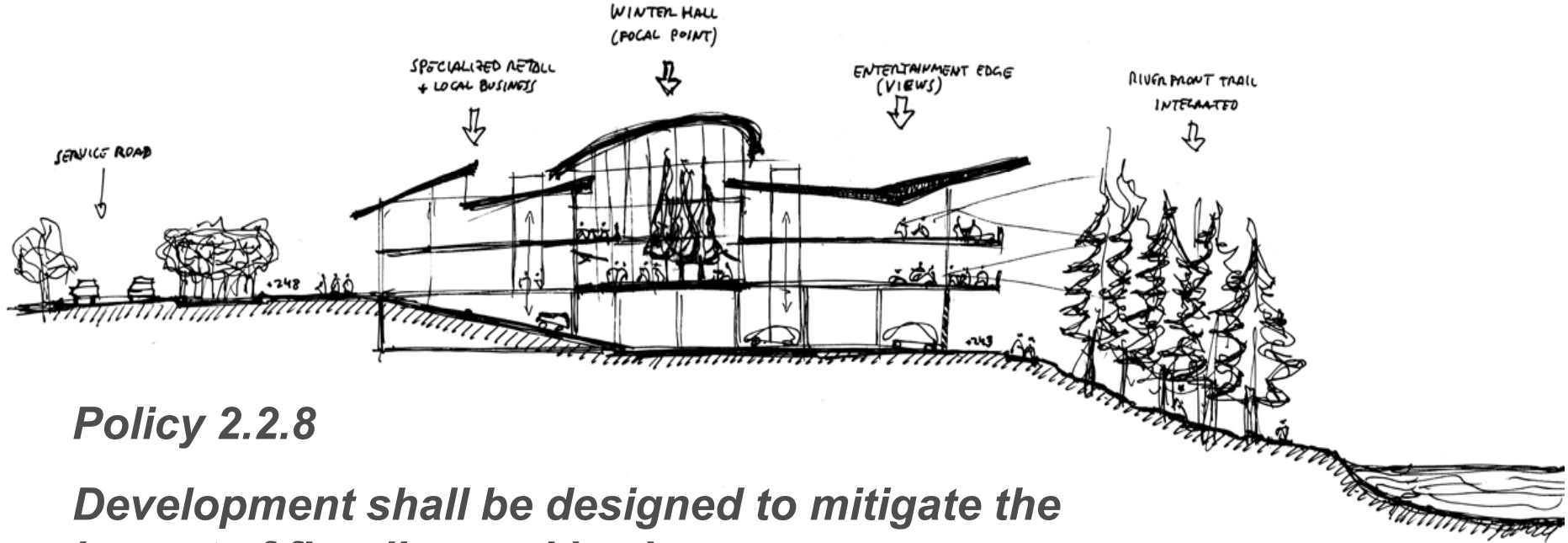


## *Policy 2.1.1*

*Development shall consist of a mix of retail development, large format retail and supporting uses.*

# Principle 2

Respect **sensitive environmental** features



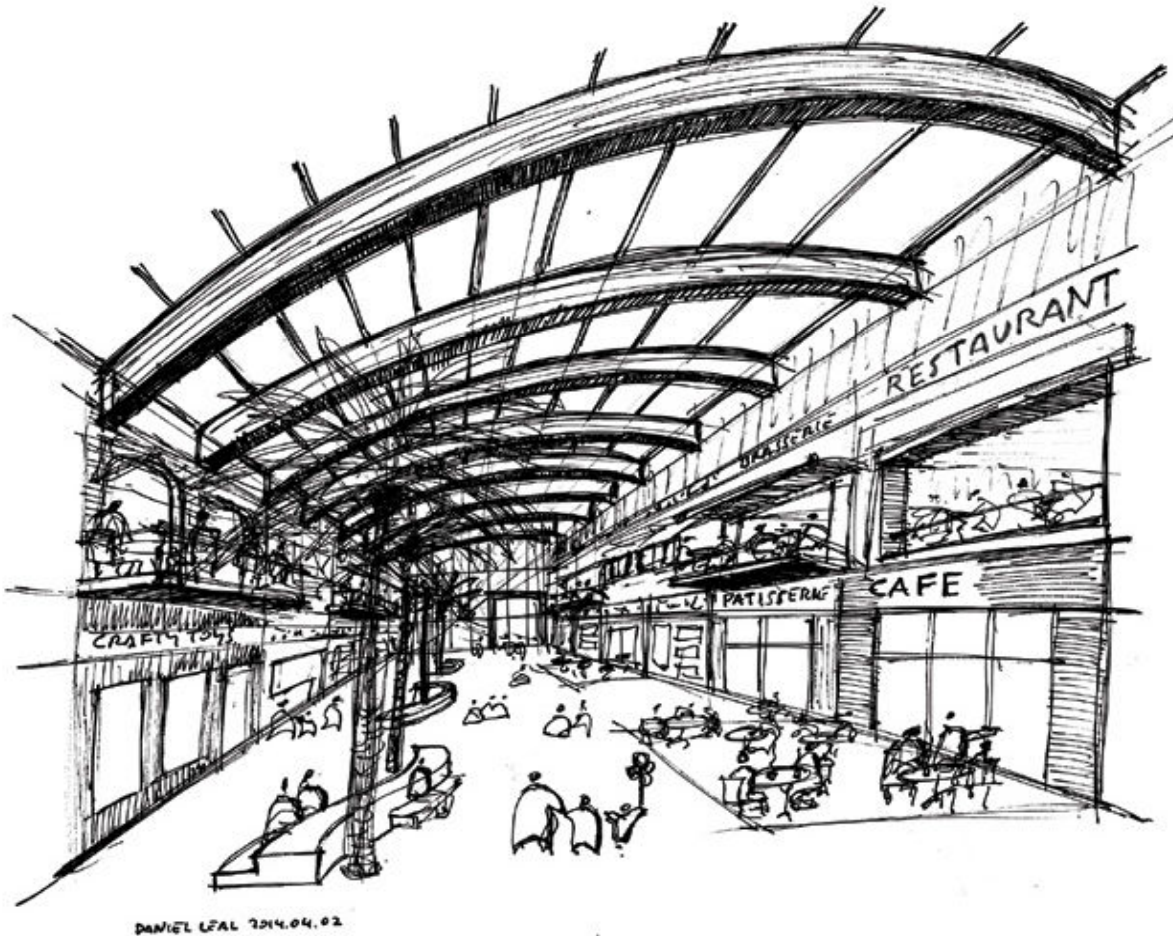
## Policy 2.2.8

*Development shall be designed to mitigate the impact of flooding and ice jams.*



# Principle 3

Create a **sense of place** through **quality public spaces** and urban design



## *Policy 2.3.9*

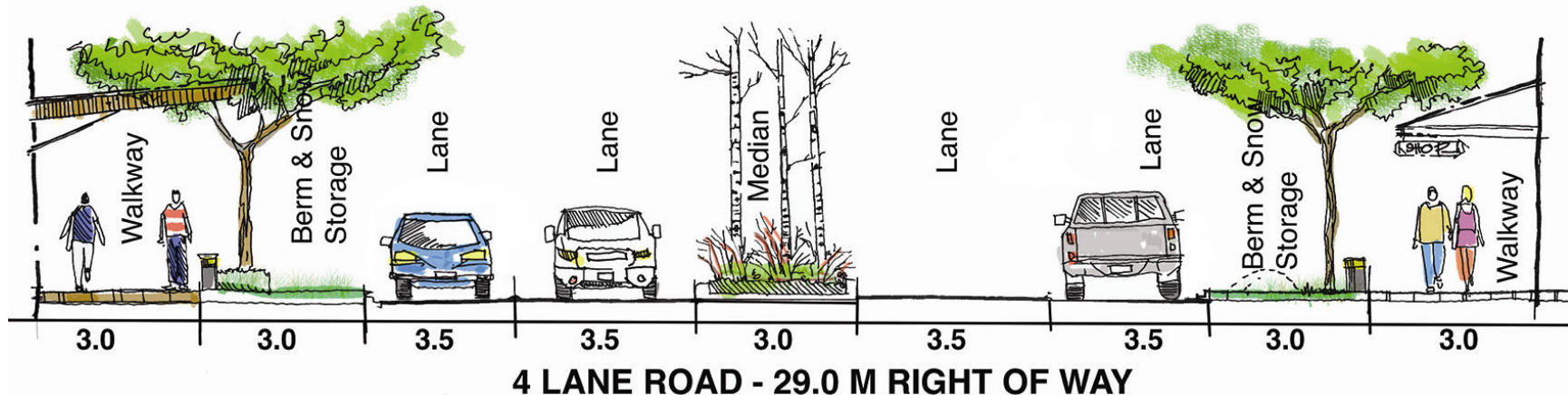
*Amenity space(s)  
shall include a  
pedestrian  
boulevard.*

# Principle 4

Provide **safe, comfortable & efficient movement** throughout the site

## *Policy 2.4.1*

*Provide safe and efficient vehicular movement between shops, amenity spaces & parking areas.*

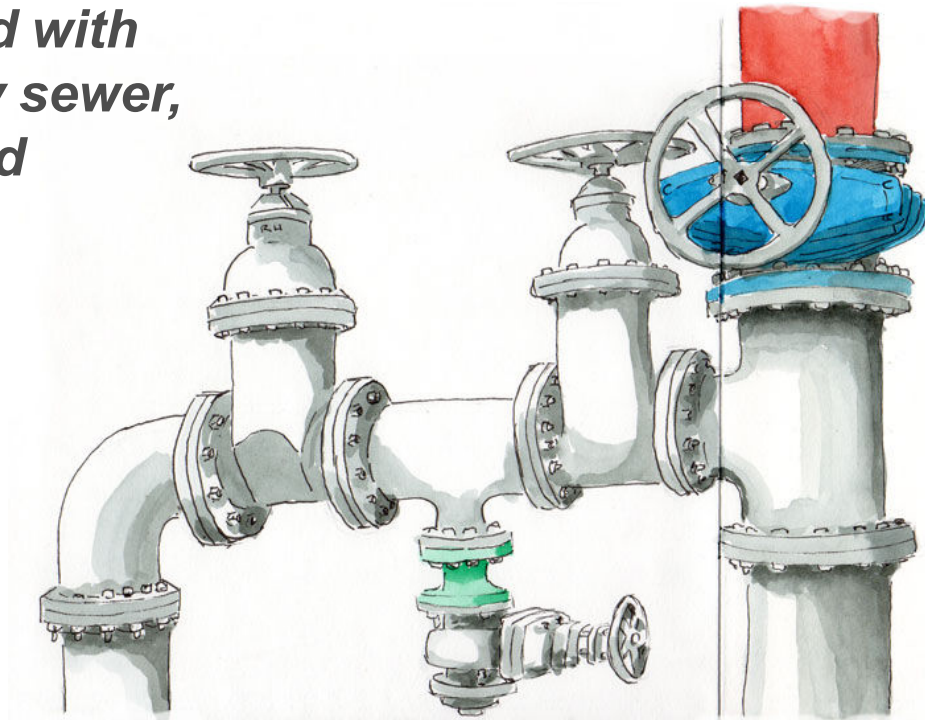


# Principle 5

Provide efficient **servicing & infrastructure** that serves the **full build-out** of the site

## *Policy 2.5.2*

*The site shall be serviced with municipal water, sanitary sewer, a stormwater system, and shallow utilities.*



# Implementing the ASP

- 1 Land Use Bylaw Amendment
- 2 Urban Service Area expansion



# Land Use Bylaw Amendment

From **Parks & Recreation (PR)**



To **Shopping Centre Commercial (C3)**



# Shopping Centre Commercial District

## Examples of Permitted Uses\*

Commercial Entertainment  
Facility

Financial Institution

Gas Bar

Office

Retail Store

Shopping Centre

## Examples of Discretionary Uses\*

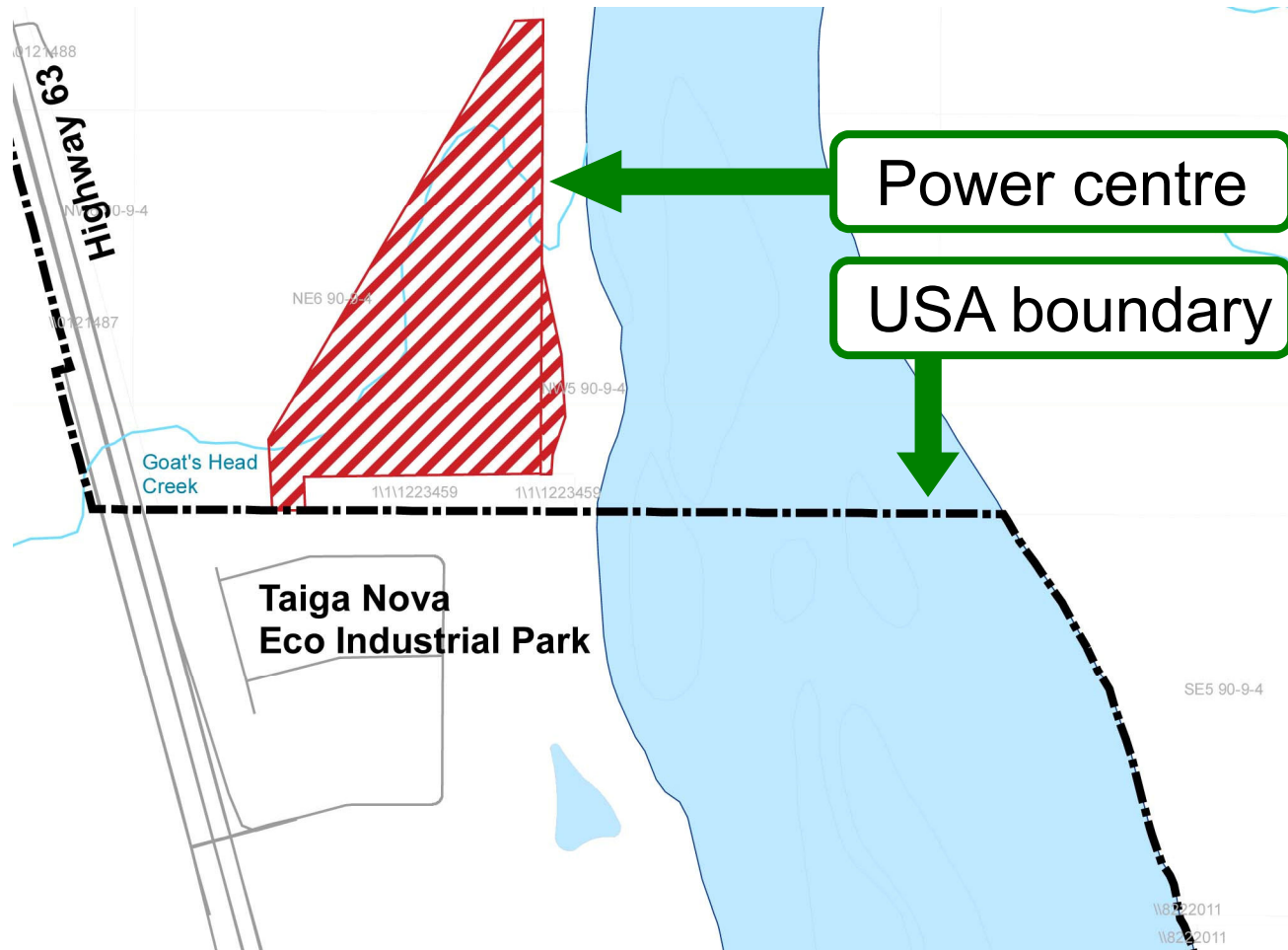
Drinking Lounge

Food Service

Hotel

*\*Complete listing found in  
Bylaw No. 99/059*

# Urban Service Area (USA) Expansion





# Thank you



<b>Subject: Land Use Bylaw Amendment – Lot 1, Block 9, Plan 132 2526 and a portion of Lot 1, Block 3, Plan 082 4071 (Longboat Landing) Bylaw No. 14/032</b>
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<b>APPROVALS:</b>
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Felice Mazzoni, Director Robert Kirby, Acting Executive Director Marcel Ulliac, Interim Chief Administrative Officer
----------------------------------------------------------------------------------------------------------------------------

**Administrative Recommendations:**

THAT Bylaw No. 14/032, being a Land Use Bylaw Amendment specific to Lot 1, Block 9, Plan 132 2526 and a portion of Lot 1, Block 3, Plan 082 4071, creating a district to be referred to as Longboat Landing Commercial District (LBL-C), be read a first time, and

THAT the required public hearing be held on September 9, 2014.

**Summary:**

An application has been received to redesignate Lot 1, Block 9, Plan 132 2526 and a portion of Lot 1, Block 3, Plan 082 4071 from the Longboat Landing District (LBL-R4) to the Longboat Landing Commercial District (LBL-C). The purpose this zoning amendment is to accommodate the opportunity to develop mixed-used commercial spaces along Prairie Loop Boulevard at the intersections of Franklin Avenue and Prairie Loop Boulevard as well as Saline Creek Parkway and Prairie Loop Boulevard.

The authority to amend the Land Use Bylaw is vested with Council under the *Municipal Government Act*.

**Background:**

In January 2011, Council approved the Longboat Landing District (LBL-R4) under Bylaw No. 10/036. The Longboat Landing District is a special land use district that sets out regulations to accommodate medium/high density residential development. However, this district only allows for minor commercial uses.

In 2012, the City Centre Area Redevelopment Plan (CCARP) was passed as Bylaw No. 12/003 and included Longboat Landing within the Riverside District of the Neighbourhood Stabilization Zone. The Riverside District in the City Centre Area Redevelopment Plan (Bylaw 12/003) Section 5.5.7.4 recommends medium to high densities around intensification nodes, specifically where Prairie Loop Boulevard intersects with Franklin Avenue and Saline Creek Parkway.

The redistricting for the Neighbourhood Stabilization Zone, as laid out in the CCARP Section 5.3 Neighbourhood Stabilization Zone has not been implemented at this point in time. As such, the existing LBL-R4 district remains in place for the site.

The landowner has now requested a land use amendment to allow for the development of commercial uses at the intersections of Prairie Loop Boulevard with Franklin Avenue and Saline Creek Parkway.

**Rationale for Recommendations:**

The proposed Longboat Landing Commercial District (LBL-C) is appropriate for the development of intensification nodes where Prairie Loop Boulevard meets Franklin Avenue and Saline Creek Parkway. The Longboat Landing Commercial District (LBL-C) is supported by the following policies outlined in the Municipal Development Plan (11/027):

1. Direction U.1.1 – Establish City Centre as focal point
2. Direction U.1.2 – Protect and enhance established neighbourhoods
3. Direction U.1.3 – Promote sustainable development in new neighbourhoods
4. Direction U.1.4 – Promote concentration of employment lands
5. Direction U.1.5 – Plan for a hierarchy of retail and commercial centres

The proposed Longboat Landing Commercial District (LBL-C) will allow for mixed use developments. Residential units will be permitted along with various commercial uses including retail, food and beverage and tourism commercial uses; however, office space will be limited so that the area does not compete with the Central Business District.

The LBL-C will be required to meet the regulations of Part 9 of the Land Use Bylaw 99/059 (the City Centre Land Use). This includes both green building standards and urban design criteria.

Administration supports the proposed amendment and recommends that Bylaw No. 14/032 be given first reading to allow for the scheduling of a public hearing.

**Attachment:**

1. Bylaw No. 14/032

**BYLAW NO. 14/032****BEING A BYLAW OF THE REGIONAL MUNICIPALITY OF WOOD BUFFALO TO AMEND LAND USE BYLAW NO. 99/059**

**WHEREAS** Section 639 of the *Municipal Government Act*, R.S.A., 2000, c.M-26 and amendments thereto authorizes Council to enact a bylaw adopting a Land Use Bylaw.

**AND WHEREAS** Section 191(1) of the *Municipal Government Act*, R.S.A., 2000, c.M-26 and amendments thereto authorizes Council to adopt a bylaw to amend a Land Use Bylaw.

**NOW THEREFORE**, the Council of the Regional Municipality of Wood Buffalo, in the Province of Alberta, in open meeting hereby enacts as follows:

1. Land Use Bylaw No. 99/059 is hereby amended by:

(a) Adding the following to Part 9, Section 9.4 DEFINITIONS:

**LIVE-WORK** includes, but is not limited to: **SECONDARY OFFICE COMMERCIAL; RETAIL COMMERCIAL; HOUSEHOLD EQUIPMENT REPAIR**; artist studio; making, processing, and assembly of products on a small scale; and **PERSONAL SERVICE FACILITY** not including dry cleaning establishments. The entire unit occupied by the **LIVE WORK** shall be considered a Dwelling Unit, consisting of Habitable Rooms and Non-Habitable Rooms. The work use must not exceed 50.0 percent of the Gross Floor Area of the Dwelling Unit and no activity which generates noise levels, dust, odors or emissions incompatible with adjacent uses shall be permitted.

**SECONDARY OFFICE COMMERCIAL** means a small scale office development designed, intended or used for the provision of professional, management, administrative, financial, health services, public service organizations, business, or similar services aimed at serving local residents, local households and locally owned businesses. Secondary Office Commercial excludes major offices that exceed a total Gross Floor Area of 5,000 m<sup>2</sup>, which are better suited to be located in the Downtown Major Redevelopment Zone. Secondary Office Commercial uses do not exceed a total Gross Floor Area of 5,000 m<sup>2</sup> within the Building or Structure in which the use is contained.

(b) Adding to 9.7.4.1 Development in Proximity to Bus Rapid Transit Stations, following the words “Prairie Loop Boulevard” the words “, the intersection of Prairie Loop Boulevard and Saline Creek Parkway.”

(c) Adding the following to 9.12.1.1 NEIGHBOURHOOD STABILIZATION ZONE DEVELOPMENT STANDARDS (BL 12/013)

f. The standards that apply to the Development on Prairie Loop Boulevard, Franklin Avenue East, and Franklin Re-Urbanization General set out in table 9.11-1 shall apply to Development in the LBL-C Longboat Landing Commercial District

- (d) Deleting **Table 9.18-1 Land Use Districts within the Neighbourhood Stabilization Zone** and replacing with the following:

**Table 9.18-1**  
**Land Use Districts within the Neighbourhood Stabilization Zone**

Land Uses	Land Use Districts	
	SR1 South Riverfront	LBL-C Longboat Landing Commercial
● = Permitted Use ○ = Discretionary Use		
Food and Beverage Commercial		●
Institutional and Civic	○	○
Live-Work		●
Office Commercial	●	
Secondary Office Commercial		●
Park	●	
Parking Lot/ Structure	●	
Residential	●	●
Retail Commercial (<5,000m <sup>2</sup> GFA)	●	●
Tourism Commercial		●
Home Occupation	●	●
Home Business	○	○
Accessory Surface Parking	●	●

<b>Land Uses</b>	<b>Land Use Districts</b>	
	<b>SR1 South Riverfront</b>	<b>LBL-C Longboat Landing Commercial</b>
Accessory Building or Use	○	○

- (e) Adding to 9.21.1 **Fundamental Design Regulations**, the words “Longboat Landing Commercial District (LBL-C)” following the words “Franklin Avenue Re-urbanization Zone”.
- (f) Following section 9.21.2.15, adding the following section, 9.21.3:

### **9.21.3 Longboat Landing Commercial Urban Design Regulations**

The regulations in this Section are required for all Development in the Longboat Landing Commercial District. In conjunction with the other applicable Development Standards, the purpose of these Design Regulations is to ensure achievement of a high quality public realm and building design.

The Longboat Landing Commercial Urban Design Regulations are as follows:

#### **Site Development Plans**

- .1 Developments within the LBL- C Land Use Area will orient buildings to minimize 100% rear exposure of buildings to Prairie Loop Boulevard. Development will incorporate regular access to municipal walkways that are abutting Prairie Loop Boulevard and incorporate design elements such as building articulation every 20.0 m to create visual breaks in the face of buildings abutting Prairie Loop Boulevard.
- .2 In compliance with Part 9 of the Land Use Bylaw the setback along Prairie Loop Boulevard is established as a maximum of 3.00 m and a minimum of 3.00 m, except the parcels identified on the City Centre Floor Area Ration Map as “Intensification Nodes” where the minimum is setback is 0 m.
- .3 Special paving, landscaping and seating must be provided to create pedestrian interest along Prairie Loop Boulevard and to promote human-scale dimensions.



- .4 Design sites and buildings to improve pedestrian comfort (microclimatic conditions) along Prairie Loop Boulevard and amenity or commercial areas. Encourage use of awnings, canopies, overhangs, colonnades, arcades and landscaping.
- .5 Create an integrated transition between public and private realm (3.00 m front and side yard setbacks) through landscape treatment and site design to facilitate pedestrian movement and interaction resulting in pedestrian walkways and public open spaces.
- .6 Pedestrian-scale lighting must be included in public-private transition areas or as an organizing element in larger surface parking areas; such as low-high lighting bollards and columns.
- .7 Additional tree planting along the curbsides in the boulevard strip between the road and sidewalk is required.

### **Regulations for Building Façade Location and Form**

- .1 The design of sites and buildings will contribute to animated streetscapes. Design buildings with articulated building facades facing public streets and publically oriented spaces.
- .2 Any development within the Intensification nodes will have windows directly facing Prairie Loop Boulevard.
- .3 Pedestrian entrances will have a more prevailing and predominant design than the vehicle entrances, to remark human-scale access and composition of façade. Some elements to reach this can be entry porches, colonnades, decks, and enhancing materials or colours around the pedestrian entrances.
- .4 Where possible, blank, flat walls will be avoided along street frontages.

### **Intensification Nodes**

- .1 Two Intensification Nodes are herein defined: one at Franklin Ave. and Prairie Loop Boulevard, and another at Prairie Loop Boulevard and Saline Creek Parkway. Within those Nodes, the height limit is 15 storeys (see Appendix 3).
- .2 The Intensification Nodes are also considered as Urban Gateways in the sense and intention defined in the City Centre Area Redevelopment Plan. As such Public Art is encouraged that is intended to narrate/relate a historical theme of Fort McMurray.

### **Parking Placements**

- .1 Surface Parking abutting Prairie Loop Boulevard is permitted but must be screened from public view from Prairie Loop Boulevard and municipal sidewalks through a landscape buffer treatment.
  - .2 Underground parking is permitted and encouraged.
  - .3 Above ground structure parking is permitted, but facades must be treated to avoid an excessive presence of car on the streetscape.
- (g) **Adding to Appendix 1** – City Centre Districts and Corridors Map, reference to the “Longboat Landing Commercial District (LBL-C)” to the “Districts” list and identifying the affected lands as shown on the attached Schedule “A”.
- (h) **Identifying on Appendix 3** – City Centre Height Map, the affected lands (Franklin and Prairie Loop Boulevard and Prairie Loop Boulevard and the future Saline Creek Parkway, as shown on the attached Schedule “A”, as lands identified as “Intensification Node – Up to 15 storeys”.
- (i) Redesignating Lot 1, Block 9, Plan 132 2526 and a Portion of Lot 1, Block 3, Plan 082 4071 from the Longboat Landing High Density Residential District (LBL-R4) to the Longboat Landing Commercial District (LBL-C) as depicted in Schedule A.
2. The Chief Administrative Officer is authorized to consolidate this bylaw.
3. This bylaw shall be passed and become effective when it receives third reading and is signed by the Mayor and Chief Legislative Officer.

READ a first time this \_\_\_\_\_ day of \_\_\_\_\_, A.D 2014.

READ a second time this \_\_\_\_\_ day of \_\_\_\_\_, A.D. 2014.

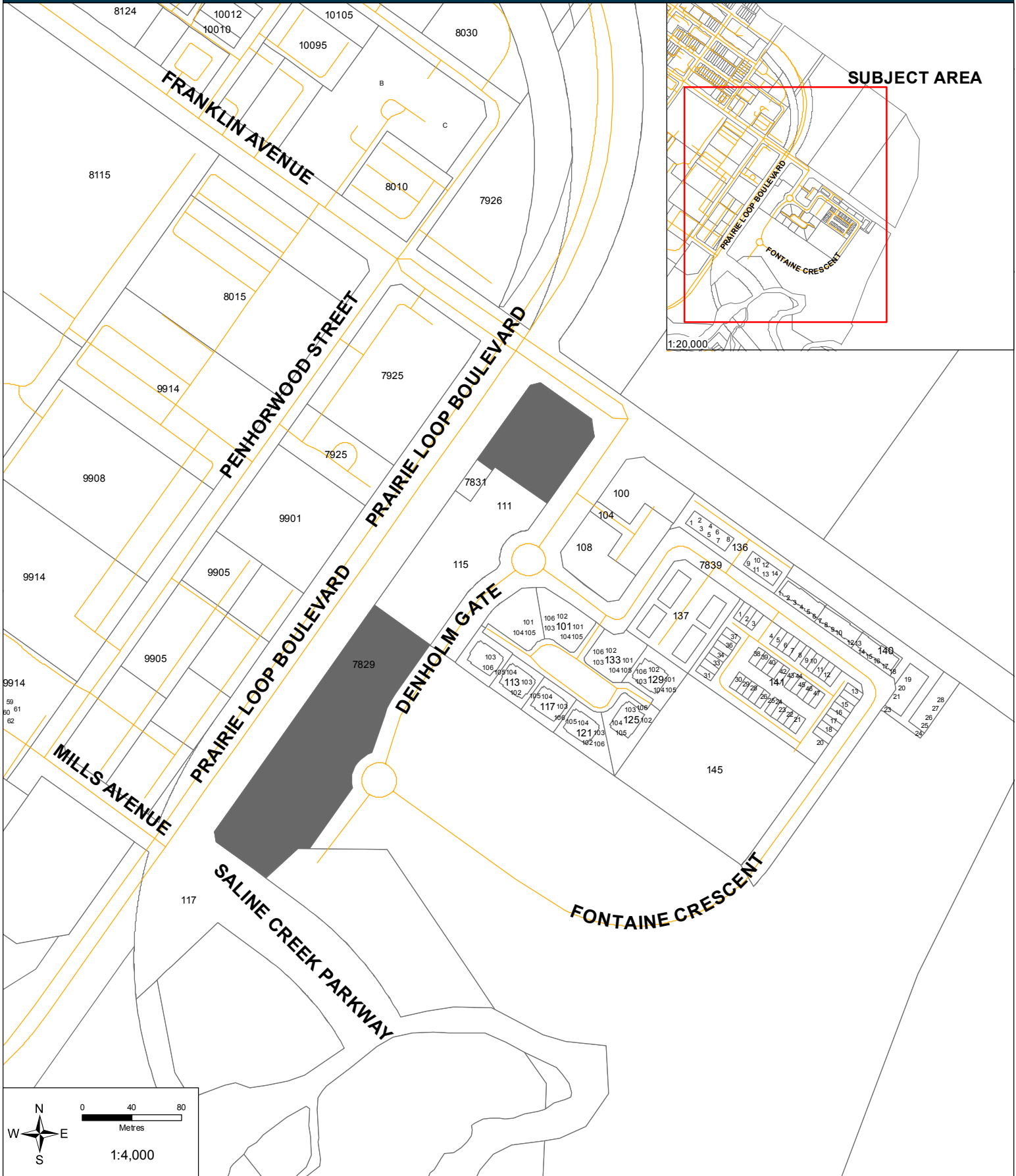
READ a third and final time this \_\_\_\_\_ day of \_\_\_\_\_, A.D. 2014.

SIGNED and PASSED this \_\_\_\_\_ day of \_\_\_\_\_, A.D. 2014.

\_\_\_\_\_  
Mayor

\_\_\_\_\_  
Interim Chief Legislative Officer





■ From LBL-R4 to LBL-C

Lot 1, Block 9, Plan 132 2526  
Lot 1, Block 3, Plan 082 4071



Map Produced by the  
Community Development Planning  
Branch on 7/28/2014

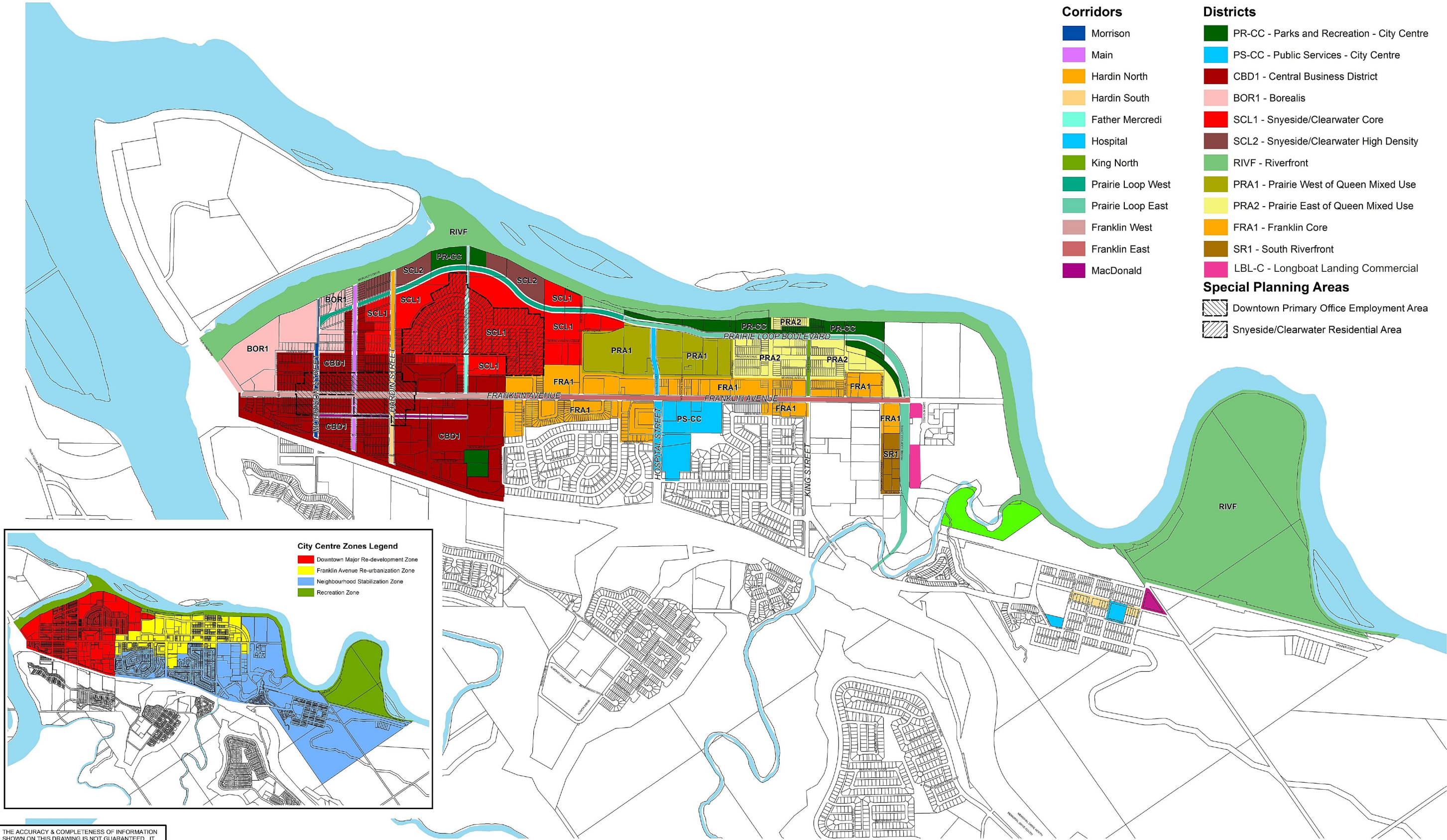






DATE: JANUARY 2013  
INFOSOURCE: REGIONAL MUNICIPALITY OF WOOD BUFFALO

REGIONAL MUNICIPALITY OF WOOD BUFFALO



U:\Projects\ELI\23750\Drawing Design Analysis\GIS\Projects\Appendix 1 Districts and Corridors - Updated Bylaw Info.mxd Last updated by Spauls on May 01/13 at 2:16:06 PM

THE ACCURACY & COMPLETENESS OF INFORMATION SHOWN ON THIS DRAWING IS NOT GUARANTEED. IT WILL BE THE RESPONSIBILITY OF THE USER OF THE INFORMATION SHOWN ON THIS DRAWING TO LOCATE & ESTABLISH THE PRECISE LOCATION OF ALL EXISTING INFORMATION WHETHER SHOWN OR NOT.





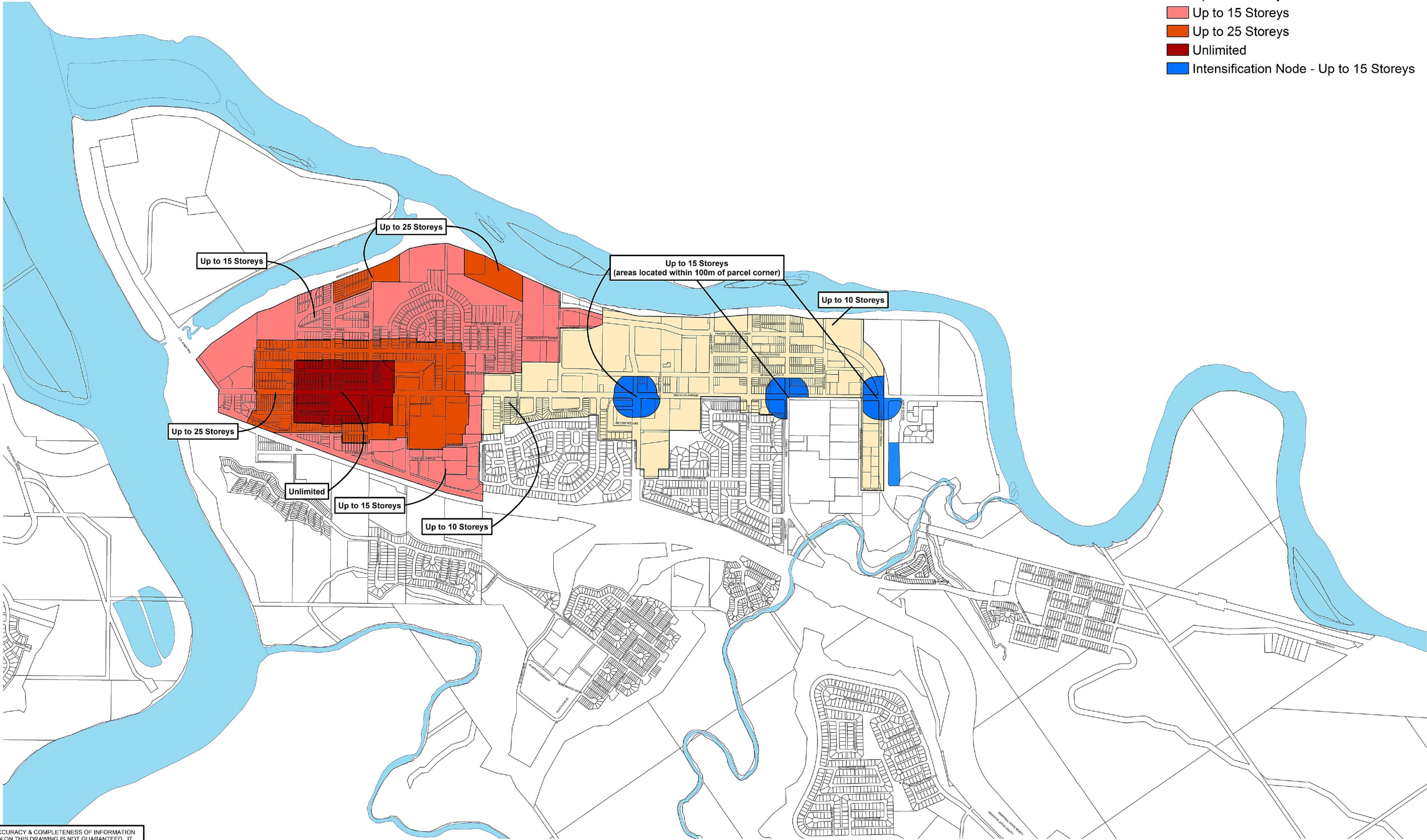




DATE: APRIL 2012  
INFOSOURCE: REGIONAL MUNICIPALITY OF WOOD BUFFALO

REGIONAL MUNICIPALITY OF WOOD BUFFALO

- Up to 10 Storeys
- Up to 15 Storeys
- Up to 25 Storeys
- Unlimited
- Intensification Node - Up to 15 Storeys



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**Subject: Council Committee Appointments****Selection Committee Recommendations:**

THAT the following appointments be approved, effective immediately:

- **Advisory Committee on Aging:**
  - Karen McMillan, Seniors Resource Committee Representative,
  - Paul McWilliams, Wood Buffalo Health Advisory Council Representative, and
  - Dave Hodson, Senior at Large Urban Representative to December 31, 2015;
  - Joan Furber, Golden Years Society Representative,
  - Bryan Lutes, Wood Buffalo Housing and Development Corporation Representative, and
  - Linda Mywaart, Senior at Large Rural Representative to December 31, 2016;
- **Combative Sports Commission:**
  - Brenda Fortais to December 31, 2015;
- **Communities in Bloom Committee:**
  - Julie Moretto to December 31, 2015;
- **Landlord and Tenant Advisory Board:**
  - Leslyn Gombakomba to December 31, 2015;
- **Library Board:**
  - Greg White to December 31, 2015;
- **Public Art Committee:**
  - Sharon Heading, and
  - Ana Maria Mendez-Barks to December 31, 2015;
  - Garry Berteig, and
  - Erin Schwab to December 31, 2016;
  - Daniel Leal,
  - Elisa Rubalcava, and
  - Carmen Wells to December 31, 2017;
- **Regional Recreation Corporation:**
  - Michelle Landsiedel to December 31, 2016;
- **Subdivision and Development Appeal Board:**
  - Sean Schaffer to December 31, 2016; and
- **Waterfront Steering Committee:**
  - Ian Dirom, Chair, and
  - Cathy Dreier, Vice Chair to December 9, 2014.

**Summary:**

The Selection Committee, which is comprised of the Mayor and Councillors Bussieres, Cardinal, Stroud and Vinni, has made recommendations pertaining to newly created Council committees and existing vacancies on various Council committees. In keeping with the established bylaws, the appointment of individuals to Council committees must be approved by Council.

**Background:**

At its July 8, 2014 meeting, Council created the Advisory Committee on Aging, the Public Art Committee and the Waterfront Steering Committee. A number of vacancies also arose on other committees following the annual appointments made in December, 2013.

The recruitment campaign for all current vacancies was conducted from July 14 to August 15, and involved advertising in the local newspapers, radio, the municipal website, social media, and community newsletters. In addition, applicants who applied throughout the year were considered for appointment. The annual recruitment campaign to fill vacant 2015 positions will begin in September 2014.

**Rationale for Recommendation:**

The appointment of members is necessary to address the newly-created Council committees and to ensure the continued viability of the various existing Council committees.

**Subject: Notice of Motion – MacDonald Island Parkade and Transit****Recommendation:**

THAT Administration, in conjunction with the Regional Recreation Corporation, develop a parking and traffic strategy for MacDonald Island, including but not limited to a parkade, enhanced transit services or other methods of transportation, and;

THAT the cost and funding options for the strategy be presented to the Audit and Budget Committee for consideration as part of the 2015 budget discussions.

**Summary and Background:**

At the Council meeting held on Tuesday, July 8, 2014, Councillor Ault served notice of his intent to bring forward the following motion for consideration at the August 26, 2014 Council Meeting:

THAT Administration, in conjunction with the Regional Recreation Corporation, develop a parking and traffic strategy for MacDonald Island, including but not limited to a parkade, enhanced transit services or other methods of transportation, and;

THAT the cost and funding options for the strategy be presented to the Audit and Budget Committee for consideration as part of the 2015 budget discussions.

In accordance with the Procedure Bylaw, the motion is now before Council for consideration.



